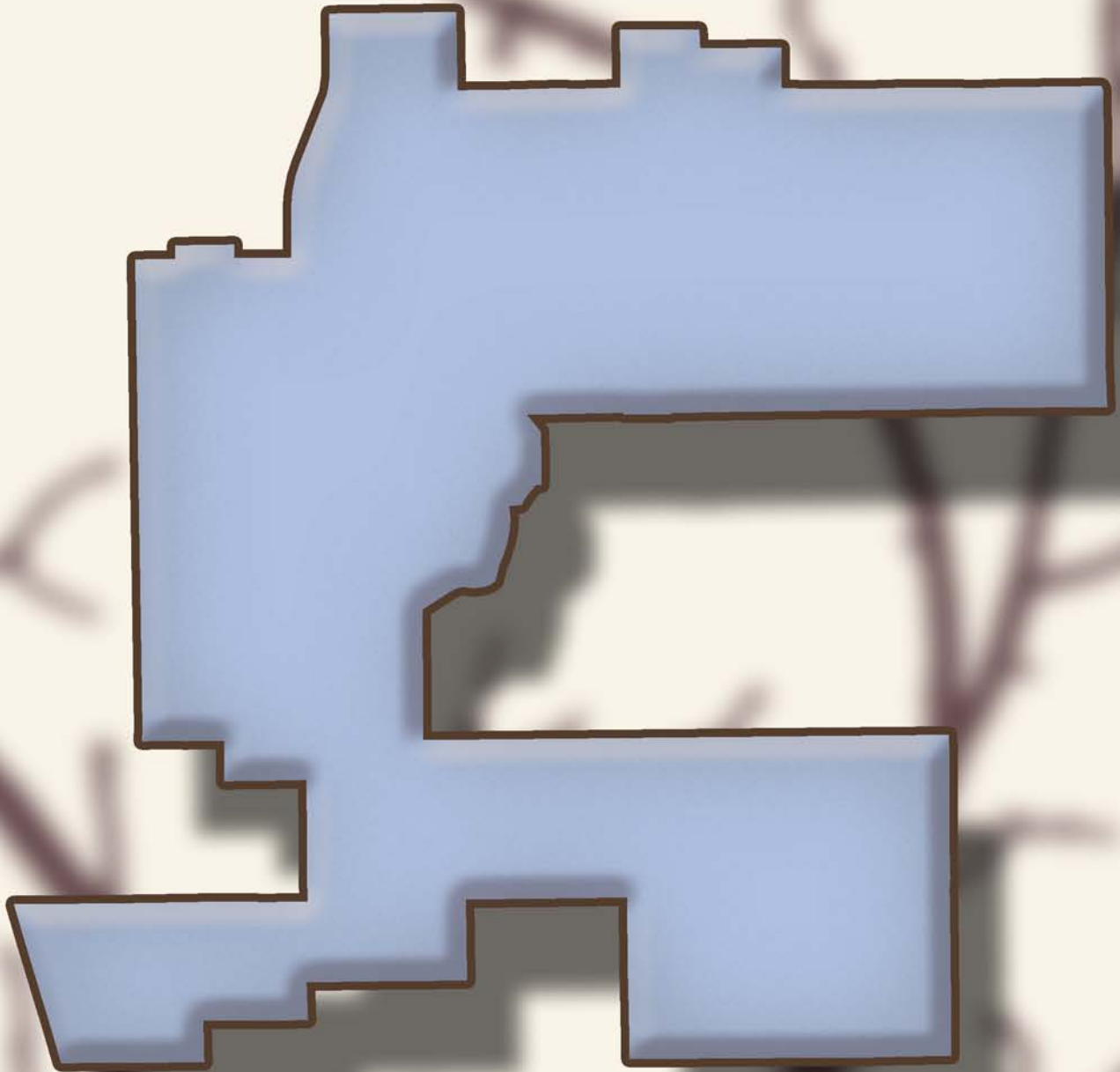


FLORIN-VINEYARD COMMUNITY PLAN



ADOPTED BY
RESOLUTION
2010-1004

COUNTY of
SACRAMENTO
December 15, 2010

ACKNOWLEDGEMENTS

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**Adopted by
Sacramento County Board of Supervisors
by Resolution 2010-1004
on December 15, 2010**

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Introduction

1.1 PURPOSE AND INTENT

In 1999, the Sacramento County Board of Supervisors initiated a community planning program for the Florin-Vineyard area, also known as the "Gap" area. The term "Gap" has been used to refer to this area because it is located between the existing urban area to the west of Elk Grove-Florin Road and a comprehensively planned urban areas to the east (i.e., North Vineyard Station and Vineyard Springs). The goal of this planning process is to provide a vision for the orderly and systematic urbanization of the study area through the establishment of a preferred land use plan and a facilities finance element based upon this plan.



aerial view of Florin and Elk Grove-Florin Roads with existing Post Office in foreground

Preservation and protection of the rural character of existing agricultural-residential neighborhoods is one of the primary objectives in the planning process. Other objectives include clustering of commercial uses at major intersections, providing appropriate locations for multifamily uses, preserving wetlands, and considering public use/transportation options for the California Central Traction Railroad tracks. The preferred land use plan shows the existing agricultural-residential neighborhoods protected by a variety of agricultural-residential and transitional land use designations. Several new urban growth areas are shown along thoroughfares and at major intersections throughout the study area. The architectural and site design guidelines included in Section 8 are intended to help guide development of this area and lessen the potential for land use conflicts. A circulation framework plan and "green streets" proposal for street sections within the plan area provide for both vehicular and alternative modes of transportation.

Other documents have been prepared as part of the community plan process. These are support documents that are separate from this community plan document and are available at the Sacramento County Planning & Community Development Department or Department of Environmental Review. These documents include the following;

- Certified Environmental Impact Report & related Appendices,
- Florin Vineyard Gap Community Plan Preliminary Drainage Study,
- Sanitary Sewer Master Plan for the Florin Vineyard Gap Community Plan,
- Master Water Study for the Florin Vineyard Community Plan,
- Florin Vineyard Community Plan Public Facilities Financing Plan.

1.2 SETTING

The Florin-Vineyard Community Plan area covers approximately 3,872 acres and is located within the community planning areas of both Vineyard and South Sacramento. The boundaries of the Florin-Vineyard Community Plan (see Figure 1.1) are generally Elder Creek Road on the north, Bradshaw Road on the east, the Churchill Downs neighborhood to the south, and the Union Pacific Railroad tracks on the west. The Florin-Vineyard Community Plan area includes approximately 670 parcels as of 2002. There are several established "ranchette" neighborhoods in this area. A total of 556 dwelling units exist as of 2002, and based on the 2000 Census, this represents a population of approximately 1,690 residents. The area is also bisected by Elder and Gerber Creeks, the Central California Traction Railroad track, and several overhead transmission lines and associated easements. Its lack of continuity is perhaps one of the biggest obstacles to a comprehensive planning effort. The large number of property owners, existing development patterns, and diverse opinions on appropriate urban development makes the Florin-Vineyard Community Plan a unique area.

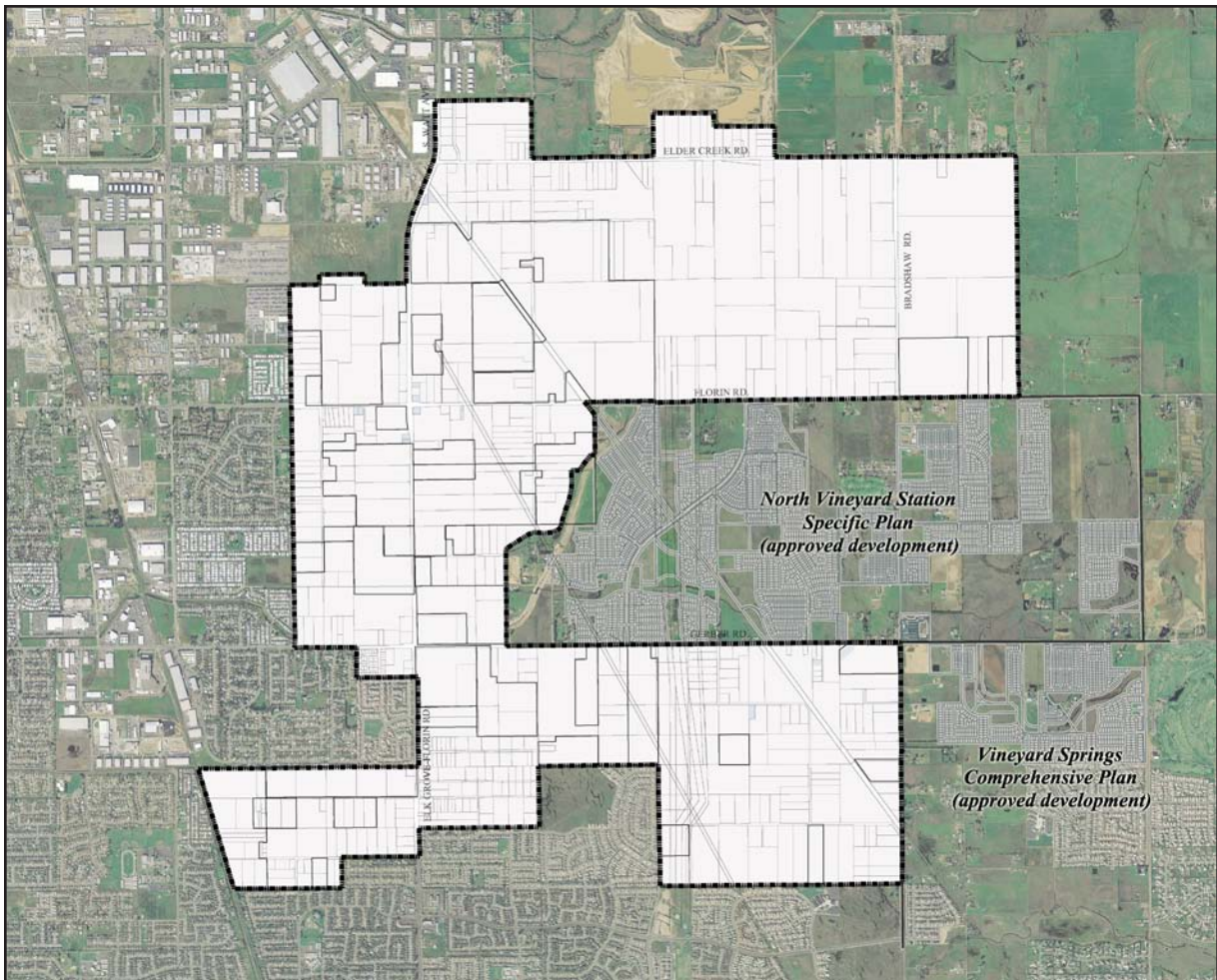


Figure 1.1 Location Map

Policy Plan

2.1 POLICY PLAN

In March, 2002, the Board of Supervisors approved formation of a Citizens Advisory Committee (CAC) for the Florin-Vineyard Community Plan. The CAC held ten workshops to discuss a number of land use issues within the plan area. The CAC expressed ideas and concerns for development within the plan area. Planning Department staff prepared policy text that was reviewed and accepted by the CAC. These policies, with modifications, have been recommended by the County Planning Commission and approved by the Board of Supervisors as a part of this Community Plan Document. The following vision, objectives, and policies apply to development in the Florin-Vineyard Community Plan.



example of small strawberry farms in the area

2.2 VISION

Provide for a high quality, clean, safe, long-lasting sustainable community that develops in an orderly and systematic manner with adequate public infrastructure and services.

2.3 OBJECTIVES

1. Establish a center focus that combines commercial, residential, civic and cultural resources.
2. Allow for continued agricultural-residential land uses to help preserve existing rural communities.
3. Provide a diversity of housing types, including a mix of sizes, types, and price ranges.
4. Promote an efficient, balanced and safe transportation system to ensure mobility for all.
5. Provide adequate sites for open space, parks, trails and greenbelts.
6. Mitigate crime by incorporating crime prevention techniques in the design of all new projects including increased visibility and integration.
7. Implementation of County code enforcement programs to prevent blight within the area.

2.4 RESIDENTIAL

Policy FV-1 Encourage a variety of lot sizes and housing types to promote social and economic diversity, and to promote greater variation visibly for neighborhoods.

Policy FV-2 Ensure an adequate mix of housing affordability dispersed throughout the community.

Policy FV-3 Promote policies to cite duplexes for all corner lots in single-family subdivisions to achieve a better integration of housing types throughout the community. Duplexes and half-plexes shall be located on corner lots. A use permit is not required for duplexes or half-plex lots that are being proposed to satisfy the County's Affordable Housing Ordinance.

Policy FV-4 Encourage larger lots for residential uses along the boundary common to any neighborhood of a lower density.

Policy FV-5 For any new residential subdivision, provide assurance (e.g., larger/deeper lots, recorded setback line, or CC&Rs) that any two-story (or greater height) structure can be set back at least 100 feet from the boundary common to an existing neighborhood of a lower density.

Policy FV-6 Promote residential development that will result in a diverse streetscape in terms of housing types, building setbacks, and building height.

Policy FV-7 The street pattern for new residential areas should be simple in design, and should provide multiple points of access (vehicles, pedestrians and bicyclists) to nearby commercial areas, parks, schools and public transit.

Policy FV-8 For any new residential subdivision, provide the Planning Department with an analysis of circulation in the larger neighborhood, and demonstrate how the proposed subdivision will contribute to and enhance circulation for area residents (vehicles, pedestrians and bicyclists).

Policy FV-9 Pedestrian and bicycle trails/pathways should be located within open space areas to the extent feasible.

Policy FV-10 The Caselman/Carlisle and McCoy Avenue areas of the Plan should maintain the existing land use pattern by including agricultural-residential land use designations in these areas. Urban residential land uses may be appropriate in the future as property owner interests and surrounding areas change. Individual applications for urban residential land uses shall be considered on a case by case basis.

2.5 COMMERCIAL

Policy FV-11 Promote mixed use concepts that capitalize on synergies between and among different types of land uses (e.g., residential and office).

Policy FV-12 Encourage the elimination of barriers to pedestrian access between different types of complementary land uses.

Policy FV-13 Promote commercial centers in new neighborhoods that are integrated into and physically connect with those adjacent neighborhoods.

Policy FV-14 Promote the incorporation of plazas and open spaces into commercial development that will provide for public gatherings and outdoor uses such as farmers' markets.

Policy FV-15 Encourage commercial architecture and building design that promotes pedestrian and various forms of multi-modal access.

Policy FV-16 Promote commercial uses that cater to nearby residents, in locations where they are easily accessible for pedestrians, and those that will help create a more active and vibrant pedestrian environment (e.g., theaters, restaurants, outdoor cafes, and farmers' markets).

Policy FV-17 Promote pedestrian-friendly, human-scale commercial development that provides safe and pleasant places for people.

Policy FV-18 Encourage screening of visibly large or tall structures such as water tanks or cellular facilities, by either locating them in areas seen by few people, or "hidden" by placement on the roof of a commercial building, or integrated into the building's design and architecture.

2.6 INDUSTRIAL

Policy FV-19 Encourage larger lots for industrial uses along the boundary common to any residential or agricultural-residential neighborhood.

Policy FV-20 For any new industrial subdivision, provide assurance (e.g., larger/deeper lots, recorded setback line, or CC&Rs) that any structure can be set back at least 100 feet from the boundary common to an existing residential or agricultural-residential neighborhood.

Policy FV-21 Outdoor industrial storage areas should be located at least 50 feet from the boundary common to an existing residential or agricultural-residential neighborhood, and this setback area should be landscaped.

Policy FV-22 Any new industrial development should either connect (extend as necessary) to public water and sewer, or provide assurance (e.g., bond, installation of dry lines, or record an agreement to connect) to connect at some future date.

Policy FV-23 Any new industrial development should comply with appropriate fire prevention measures, including an adequate water supply for necessary fire flows.

Policy FV-24 Promote the use of the industrial area along Elder Creek Road for future employment, office and business uses.

2.7 OPEN SPACE

Policy FV-25 Residential development adjacent to drainage parkways should avoid providing an excessive number of back-up lots, and should provide front-on street and open ended cul-de-sacs whenever possible.

Policy FV-26 Improvements to drainage parkways should be consistent with the most recently approved plans and/or improvements (downstream or upstream) in terms of width, landscaping, and pedestrian access.

Policy FV-27 Promote the construction of a network of pedestrian/bicycle trails within the study area, particularly within the electrical transmission corridor that runs north-south through the study area, near the alignment of Waterman Road.

Policy FV-28 Residential development adjacent to the electrical transmission corridor (running north-south through the study area, near the alignment of Waterman Road) should avoid providing an excessive number of back-up lots, and should provide front-on streets and open ended cul-de-sacs whenever possible. In addition, residential fencing separating yard areas from this electrical transmission corridor should be an open design to allow viewing of the corridor.

Policy FV-29 Identify a funding source that would allow for the Southgate Recreation and Park District to take ownership and assume maintenance responsibility for the electrical transmission corridor running north-south through the study area, near the alignment of Waterman Road.

Policy FV-30 Ensure no net loss of vernal pool acreage, and/or values and functions, and mitigate any loss in relation to the values of quality of habitat.

Policy FV-31 Evaluate feasible on-site alternatives in the environmental review process that reduce impacts on vernal pools and provide effective on-site preservation in terms of minimum management requirements, effective size, and evaluation criteria.

Policy FV-32 Pedestrian and bicycle trails/pathways should be located within open space areas to the extent feasible. Open space areas should include an adequate buffer around preserve areas adjacent to residential lots and/or streets to allow for pedestrian and bicycle trail/pathway improvements.

2.8 STREETSCAPE

Policy FV-33 Prominent entry subdivision signage and landscape treatment is encouraged at project entry points to provide a greater sense of neighborhood identity.

Policy FV-34 Median landscaping is encouraged for all thoroughfares, arterials, and collector streets.

Policy FV-35 All wood fences and masonry walls should be screened with trees and shrubs. Chain-link fencing is discouraged. Open-type fencing (e.g., wrought-iron fencing) should be used adjacent to all open space areas. Fences shall be located along the property lines in order to limit or avoid any future property owner disputes or nuisances.

Policy FV-36 Soundwalls within the Plan area shall be designed consistent with the materials and colors described in the Community Themes portion of the Design Guidelines section of this plan. Where possible, land use and site design should be considered that avoid use of soundwalls.

Policy FV-37 Landscaped corridors should be included adjacent to the right-of-way on both sides of all thoroughfares, arterials, and collector streets. The width of this corridor should be a minimum of 25 feet on both sides of thoroughfare and arterial streets, and 15 feet on both sides of collector streets.

Policy FV-38 Pedestrian facilities along thoroughfare and arterial streets should include a 6-foot wide sidewalk, separated by the curb by a variable width planter that includes trees. Pedestrian facilities shall conform to the standards contained in the document and design requirements of the County Improvement standards. Sidewalks should be linear within the corridor and meandering sidewalks avoided to the extent possible.

Policy FV-39 *Project circulation design shall be consistent with the Green Streets Framework Plan, Street Sections, and Bicycle and Trail System found in the Transportation Section of this Plan.*

Policy FV-40 *Street lights shall be provided on all streets within the Plan area in accordance with Sacramento County Improvement Standards. Energy efficient and decorative lighting standards above County requirements is encouraged where practicable.*

2.9 PUBLIC FACILITIES

Policy FV-41 Construction of school sites within the Florin Vineyard Community Plan shall be consistent with the following elements;

- a. Provide the number, type, design, and location of school facilities consistent with the Elk Grove Unified School District Master Plan.
- b. New elementary school sites should be designated adjacent to existing or proposed neighborhood or community park sites and be designed to promote joint use of both facilities.
- c. Provide bikeways or pedestrian facilities to link school sites with residential areas. These facilities shall connect to the overall trail system and green street concepts planned within the planning area.
- d. New elementary schools should have frontage on two adjacent streets.

Policy FV-42 Prior to approval of a final map for any residential subdivision map in the Florin Vineyard Gap Community Plan area, the applicant shall obtain written confirmation from the Superintendent of Elk Grove Unified School District ("EGUSD") or his/her designee that school enrollment impacts have been or will be adequately mitigated through the reservation (per

Policy Plan

California Government Code Section 66479 and 66480) of a school site or sites or through other measures. The reserved site or sites must be in a location and configuration acceptable to the EGUSD, consistent with the conceptual school site locations identified on Exhibit 7.3 of this plan, and, to the extent practicable, consistent with the school siting policies set forth Section 7.4.

Policy FV-43 Park facilities and sites shall be provided consistent with the Southgate Recreation & Park District standards and policies. Park sites within the Community Plan shall be consistent with the following elements;

- a. Parkland dedication shall be determined in accordance with Sacramento County Title 22 requirements;
- b. Parks shall be located and sized to meet the recreational needs of the plan area and surrounding neighborhood;
- c. Parks should be bordered on at least two sides by streets, preferably three, in order to facilitate public access and visibility;
- d. Whenever possible, park sites should be located adjacent to public facilities such as schools, libraries, fire stations, and open space corridors. Joint use agreements should be encouraged. In such instances, recreation amenities, including play equipment and athletic fields, should be shared to minimize cost;
- e. Park sites adjacent to stormwater detention and/or treatment facilities are encouraged. In these cases, a pedestrian trail system should be included within the two facilities. Consideration for joint-use of park and recreation facilities within the stormwater facilities should be included early in the project design phase.

Policy FV-44 The drainage parkway of Elder Creek and Gerber Creek as identified on the land use plan shall be designed as natural-appearing corridors serving to enhance wetlands and riparian habitats, act as natural flood water detention areas, and function as water quality enhancement features. The drainage parkways shall be designated to permit the growth of vegetation that does not impede the design flow characteristics. Periodic clearing is allowed to maintain channel function. At least one side of the drainage parkway shall have a single-loaded street with front-on lots toward the corridor. Backup lots along the drainage parkway is discouraged.

Land Use Summary

3.1 LAND USE SUMMARY

The plan provides for the ultimate development of the approximately 3872 acre study area, including 9,919 dwelling units in a wide range of types and densities, retail commercial, industrial, business and commercial uses, parks and open space, and schools. Following are some of the key features of the plan:

- A primarily residential community served by the necessary services (e.g., commercial services, parks, and schools), and public infrastructure.
- A range of residential and agricultural-residential densities to help provide a better transition between some of the existing and proposed neighborhoods, and to provide for a wide range of housing types and densities.
- Commercial and/or mixed use centers at major intersections that will help provide retail commercial shopping and employment opportunities for area residents and create community focus areas.
- Open space dispersed throughout the community to serve both active and passive recreational needs.
- School, park, and other public facilities to serve the needs of the community.
- A railroad corridor identified for possible future public transit use or a pedestrian/ bicycle trail.
- A network of drainage parkways that is part of a larger, county wide solution to area wide flooding.
- A network of pedestrian and bicycle pathways within designated roadways, drainage parkways, and open space areas.
- Land use policies and design guidelines that will help guide development and lessen the potential for land use conflicts.



aerial view of the Caselman Road/ Carlisle Avenue area

3.2 LAND USE PLAN

The adopted land use plan applies to all properties within the Florin Vineyard Community Plan and supersedes the existing South Sacramento and Vineyard Community Plans. Land use areas shown are based on a GIS parcel base. Actual property lines and land use areas may vary. Where inconsistencies occur, the Planning Department staff shall determine land use lines using more accurate information as described in Section 3.3.

Figure 3.1, Land Use Plan, shows the location, types and intensities of land uses within the plan area. Table 3.1, Land Use Summary, includes an accounting of the land use designations by acreage and potential dwelling yield.

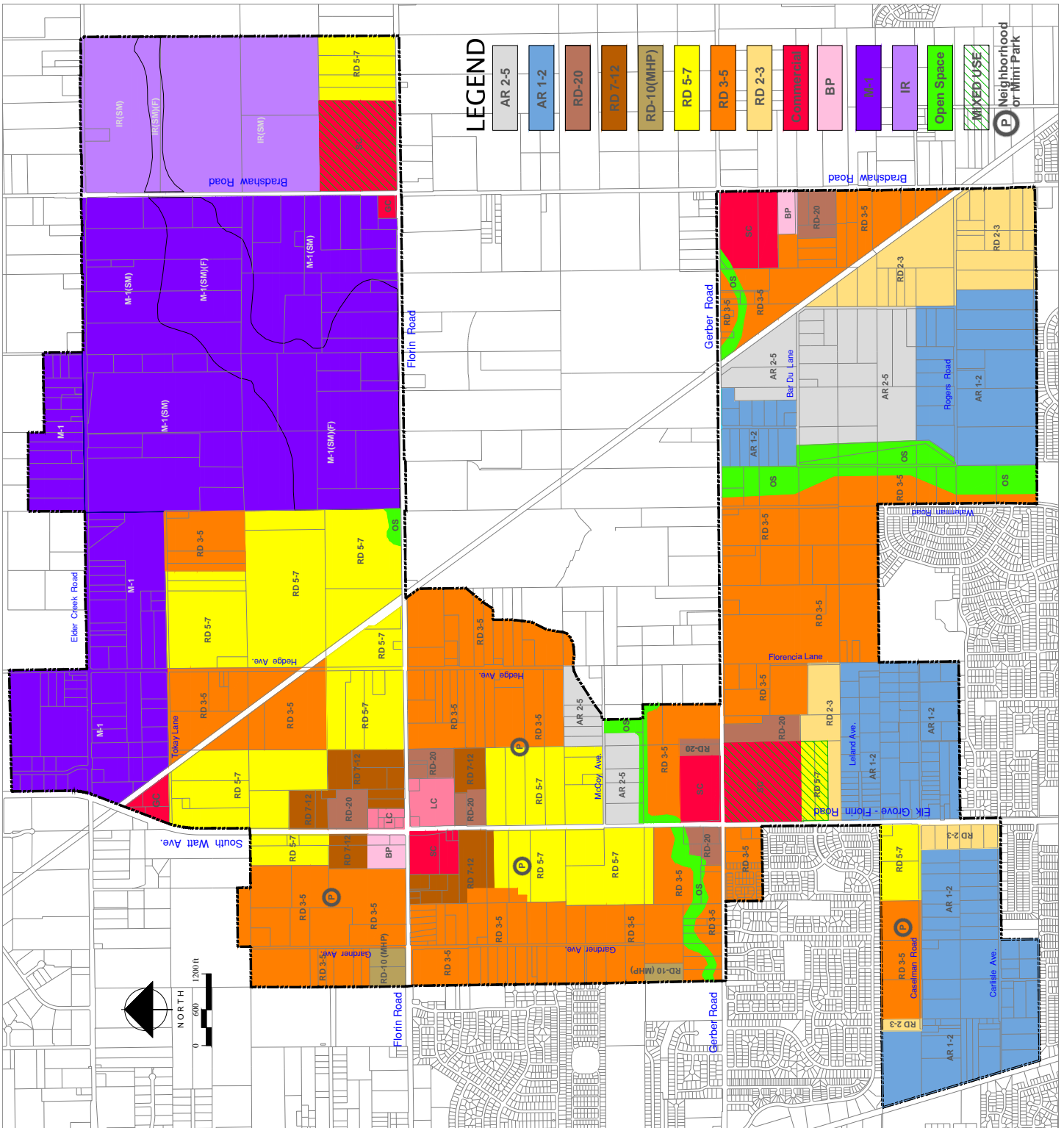


Figure 3.1 Land Use Plan

Note: Total units shown use an assumed density that may vary with individual projects. Factors such as location, street design, utilities and drainage improvements will determine actual density yields.

Land Use Type	Allowed Density	Acres	Units	Assumed Density (du/ac)
AR 2-5	2 to 5 ac/du	169.1	55.8	0.33
AR 1-2	1 to 2 ac/du	448.8	296.2	0.66
RD 2-3	2 to 3 du/ad	145.0	362.5	2.5
RD 3-5	3 to 5 du/ac	918.5	4,133.3	4.5
RD 5-7	5 to 7 du/ac	519.9	3,119.4	6.0
RD 7-12	7 to 12 du/ac	74.6	746.0	10.0
RD-10(MHP)	10 du/ac	13.7	137.0	10.0
RD-20	15 to 20 du/ac.	59.4	1,069.2	18.0
Residential Subtotal		2,349.0	9,919.4	
M1		885.3		
IR		237.6		
GC		10.2		
SC		140.6		
LC		18.5		
BP		13.3		
Open Space		98.1		
Major Roads & CTRR		119.4		
Grand Total		3,872.0	9,919.4	

Table 3.1 Land Use Summary

3.3 CALCULATION OF LAND USE

The Florin-Vineyard Community Plan does not include specific dwelling unit allocations for each property. Instead, the designated land use is applied to each property and determined on a case by case basis. To determine the allowable dwelling units per this Community Plan, the following method shall be applied;

1. Land use designations from the Sacramento County GIS parcel base is applied to the subject property using best available boundary information. In cases where the GIS base and boundary information differ, a "best fit" method shall be applied using known road centerlines or property corners.
2. Areas of land use designations are calculated from the boundary information and multiplied with either of two densities described below to determine project specific dwelling unit calculation.
 - A. If a project does not propose density averaging described in Section 3.4, then the maximum density shown in the applicable Density Range shall apply. For example, a project with a RD 3-5 designation shall use the maximum density of 5 du/ac density for calculation.
 - B. If a project proposes density averaging described in Section 3.4, then the Assumed Densities shown on Table 3.1 shall be applied. For example, a project with a portion designated RD 3-5 shall use the assumed density of 4.5 du/ac for calculation.

For the purposes of this Section, single family residential land use area is based on gross acreage. In such instances, the acreage includes land to the centerline of the adjacent existing or planned roadways. Land use areas for multi-family, commercial, office and industrial shall be based on net of existing or planned public roadways.

Project applications shall be reviewed by Planning Department staff against the above land use calculation method to determine consistency with the approved Florin Vineyard Community Plan Land Use Map.

3.4 DENSITY AVERAGING

Generally, residential development will occur within the density ranges specified in Table 3.1. However, in order to promote housing diversity and accommodate housing on sites with unusual physical conditions, landowners may elect to deviate from the density range in accordance with the following provisions described below. A density averaging proposal shall not exceed the total calculation of land use described in Section 3.3. An exception is allowed for affordable housing density bonus described in Section 3.6.

Density averaging is limited to a maximum of one dwelling unit per acre increase above the specified land use designation or range. In cases where identified land use designation is a range, the one category increase may be applied to the high end of the range. For example, the maximum allowable density in an area designated RD 3-5 would be RD-6. The one category increase can only be applied up to the RD-7 category. For example, based upon the density averaging option, the high end range for the RD 5-7 range is RD-7, while the high end range for the RD 3-5 range is RD-6. Similarly, the high end range for the RD 2-3 range is RD-4. For a project with multiple ranges (i.e. a combination of RD 2-3 and RD 3-5), a unit category increase is permitted (RD-6) throughout the entire project site as long as the overall project count does not exceed the total calculation of land uses described in Section 3.3.

Densities lower than the range shown in Table 3.1 for single family residential land use are also permitted up to a maximum of one dwelling unit per acre lower than the specified land use designation or range. For example, the minimum allowable density in an area designated RD 3-5 would be RD-2. An exception applies to parcels created to preserve existing houses within a proposed subdivision. In this case, if a minimum 1-acre lot is proposed, a minimum density of 1 du/ac shall be allowed only for that lot.

Density averaging proposals shall be made with application of a rezone or other application what requires approval by the Board of Supervisors. An exhibit illustrating and tabulating any proposed density averaging shall be submitted to the County concurrent with any corresponding tentative map or development application. Density averaging done in accordance with this section is deemed consistent with the Florin-Vineyard Community Plan and is permitted without a community plan amendment.

3.5 AFFORDABLE HOUSING - LAND USE CONSISTENCY

Development of affordable housing is required in Chapter 22.35 of the County Code. The Ordinance requires 15% of a project to meet affordable housing criteria. The Ordinance allows a number of options to meet the affordable housing requirements including on-site construction, land dedication of multi-family land to the Sacramento Housing & Redevelopment Agency (SHRA), or payment of in-lieu fees. Each development project may choose from affordable housing options, within certain limits.

If a project proposes to build on-site multi-family units or dedicate land to SHRA, a site must be zoned RD-20. Typically, these sites are 4 to 10 acres in size and are located along major roadways. Since each project must meet the requirements of Chapter 22.35, such sites may be located on land that is not designated RD-20 in Figure 3.1, Land Use Plan. As a result, an affordable housing site with RD-20 zoning may be allowed on any land use designation of RD 2-3 or above and shall be considered consistent with the Florin-Vineyard Community Plan. In these cases, the proposed affordable housing site should be located consistent with General Plan policies and Affordable Housing Ordinance requirements. This provision does not apply to multi-family residential projects with market-rate housing.

3.6 AFFORDABLE HOUSING - DENSITY BONUS

A density bonus for affordable housing is allowed in Chapter 22.35 of the County Code. The formula for calculating a density bonus is established by State law and is based on amount and type of proposed affordable housing. A density bonus of 15% to 30% is possible in many cases. This density bonus may be applied anywhere within a proposed project. For example, a density bonus from an affordable multi-family portion of a project may be applied to other single-family portions of the project. An affordable housing density bonus may result in housing types with densities above that shown in Figure 3.1 Land Use Plan. In these cases, such land uses shall be considered consistent with the Florin-Vineyard Community Plan.

3.7 UNDERLYING COMMUNITY PLAN LAND USE DESIGNATION

Lands subject to dedication and/or reservation as park and school sites are assumed to have an underlying community plan land use designation which is compatible with surrounding uses. In the event a park or school site is either reduced in size or eliminated due to a documented change in the controlling agencies plans, a lessening of dedication requirement, or other reason approved by the Board of Supervisors, the underlying land use designation shall apply and is permitted without a community plan amendment.

3.8 CODE ENFORCEMENT

The Florin Vineyard Gap area consists of several older neighborhoods that may be in need of rehabilitation and structural improvement. The Code Enforcement Division of the County's Department of Neighborhood Services is the designated entity for enforcing local and state adopted codes and ordinances to ensure compliance for safe living in the unincorporated area of the County. The Code Enforcement Division currently offers a comprehensive program to protect a property owner's investment, promote public health and welfare, and enhance the quality of life in Sacramento County. Code Enforcement staff works to achieve voluntary compliance through notification and education. When necessary, staff will use legal procedures including boarding structures, removing junk and rubbish and junk vehicles, civil citations, criminal citations and demolition of dangerous buildings. The newly enacted Rental Housing Inspection Program provides a more regulated approach to insuring safe rental properties within unincorporated Sacramento County.

Residential Land Use

4.1 RESIDENTIAL LAND USE

The Florin Vineyard Community Plan includes a mix of housing densities from agricultural-residential to multi-family residential. Single family residential densities from 2 to 12 dwelling units per acre allow a range of housing types within the planning area. RD-20 land use allows for higher density uses located within community focus areas and along major roadways. Generally, the higher residential density land uses are oriented along the major roadways and toward the Community Focus Areas with lower densities further away.



4.2 AGRICULTURAL-RESIDENTIAL LAND USE

The agricultural-residential land uses are intended to preserve existing uses and allow continued agricultural-residential uses in the future. These areas include the Bar Du Lane/ Rogers Road area between Waterman Road and Bradshaw Road, and the Lealand Avenue area east of Elk Grove-Florin Road. Conversion of these areas to urban uses is possible in the future but should only be considered in a comprehensive fashion. Sacramento County has adopted a "Right to Farm" Ordinance to implement measures to protect farming activities in these areas. Utility services to agricultural-residential uses may be limited to individual well and septic. Public water service may be extended to these areas for lots generally less than 2 acres in size.

4.2 ZONE CLASSIFICATIONS

The zone classifications on residential property within the Plan area are those of the standard Sacramento County Zoning Ordinance. Zone classifications that may be applied by rezone action to properties within the Plan area shall be consistent with the Community Plan designations.

4.3 DEVELOPMENT STANDARDS

The development standards for areas designated residential or agricultural-residential as shown on Figure 3.1 are described in Chapter 5, Article 1 of the Sacramento County Zoning Ordinance.

One exception to County Zoning standards is the setback for fencing in the residential designations. Fence setbacks in the residential designations shall supersede the County Zoning Code and shall be subject to the following;

Fence setbacks

Side street setback, for any size fence	5 feet
Front setback- fences above 4 feet in height .	20 feet
Front setback- open fences and 4 feet high or lower.	0 feet

4.4 PERMITTED USES

Permitted uses within areas designated as residential land uses as shown in Figure 3.1, Land Use Plan, are those uses that are permitted in accordance with the zoning for the property in existence on the date this plan was adopted and those permitted uses described in Chapter 1, Article 1 of the Sacramento County Zoning Ordinance for the AR-2, AR-1, RD-1, RD-2, RD-3, RD-4, RD-5, RD-7, RD-10, RD-15, RD-20, and RD-25 zoning classifications, subject to the special conditions specified and contained herein. Notwithstanding, the permitted uses associated with the zoning on property in existence when this plan is adopted remains in effect until the property is rezoned.

Commercial Land Use

5.1 COMMERCIAL LAND USE

The Florin Vineyard Community Plan includes a limited amount of commercial and business/professional land use primarily for the convenience of future residents of the Community Plan and nearby areas. Commercial development designated within the Plan area is not intended to meet all of the regional commercial needs of the Plan area residents and is not intended to include major shopping and employment opportunities for the larger South County area. Instead, the Plan provides a limited percentage of the total land area for commercial uses. The planned commercial uses will serve the frequently recurring needs of area residents for commercial goods and services.



The approximate 183 acres of commercial use is distributed among five main areas within the Plan area. Each commercial area is located at the intersection of major roadways. These areas will be easily accessible to possible future public transit routes. The main commercial areas, located at major intersections, are spaced at least one mile apart and provide reasonable spacing to serve the surrounding community. The Plan includes higher density residential land uses around the commercial areas to create community focus areas and maintains appropriate transitions of land use density. These areas are an important part of the community themes for the Florin Vineyard Community Plan. Additional discussion of community themes can be found in Section 8.5 of this Plan.

5.2 ZONE CLASSIFICATIONS

The Zone Classification that may be applied by rezone action to properties within the Plan area designated Commercial or Business/Professional on the land use plan includes Shopping Center (SC), Limited Commercial (LC), General Commercial (GC) and Business/Professional (BP). The Floodplain (F) combining zone classification must be applied to property that is subject to flooding. These zoning classifications on property within the Plan area, and in existence at the date of this plan adoption, are consistent with Commercial and Business/Professional land use designation in this Plan.

Other commercial, business/professional, and/or residential zone classifications may be found consistent with the Commercial or Business/Professional land use designations in the land use plan if those classification are used for mixed use development and are found to be consistent with General Plan and Community Plan policies. Determination of such consistency shall be recommended by Planning Department staff and approved by the County Board of Supervisors.

5.3 DEVELOPMENT STANDARDS

The development standards for areas designated Commercial or Business/Professional as shown on Figure 3.1, Land Use Plan, are described in Title III, Chapter 15 of the Sacramento County Zoning Ordinance.

5.4 PERMITTED USES

Permitted uses for areas designated Commercial or Business/Professional as shown in Figure 3.1, Land Use Plan, are those that are permitted in accordance with the zoning for property in existence on the date this plan was adopted and those permitted uses described in Title II, Chapter 25 of the Sacramento County Zoning Ordinance, subject to the special conditions specified and contained herein. Notwithstanding the permitted uses associated with the zoning on property in existence when this plan is adopted remains in effect until the property is rezoned.

5.5 COMMERCIAL/ RESIDENTIAL MIXED USES

It is the intent of this Plan to allow for mixed use development within the Plan area where appropriate opportunities apply. The commercial and multifamily areas shown in Figure 3.1, Land Use Plan, particularly at the Community Focus Areas, are likely candidates since they are located at major roadway intersections, however other mixed use development locations are possible within the Plan area.

Mixed use development of retail, office, employment and residential uses, both vertically and horizontally integrated, is a way to create active, urban environments that creates a sense of place and encourages more pedestrian uses within the larger neighborhood. Important design features of mixed use development such as building placement, public space, and pedestrian-friendly streets help to create a more livable environment. To accommodate and encourage this type of development, the land use plan designates several sites for "mixed Use" centers with a land use overlay.

The development of mixed use development is encouraged within the Florin Vineyard Community Plan but is not mandatory. Location and design of a mixed use project should be considered in relation to the surrounding land use context and may not be appropriate in all of the designated commercial areas. The land use plan designates the following two sites for mixed use development- northeast corner of Florin and Bradshaw Roads, and southeast corner of Gerber and Elk Grove-Florin Roads. On these sites, subsequent review of development plans shall place a strong emphasis on mixed use concepts to create a sense of place and encourage pedestrian activity.

Mixed use development proposals, and needed general plan amendment and rezone entitlements, shall be considered consistent with the Florin Vineyard Community Plan, provided the project is consistent with the policies of this Plan and the County General Plan, and is compatible with the existing and future neighborhood.

Industrial Land Use

6.1 INDUSTRIAL LAND USE

The northeast portion of the Plan area is designated Light Industrial (M-1) and Industrial Reserve (IR). Existing land uses at the time of plan adoption includes a mix of plant nurseries and other agricultural use, residential-agricultural sites, light industrial uses, electrical substation and transmission lines. This area of the Plan is also constrained by the Elder Creek floodplain, wetland resources, and an aggregate resource overlay. The area also lacks major sewer and water services.



The ability to develop large industrial areas in the northeast portion of the Plan area is unlikely due to the number of constraints described above. The Jackson Road Visioning Plan concept map shows a mix of residential use and neighborhood centers. Some additional industrial development is possible but will likely be limited in size and scope. For these reasons, the industrial land uses in this plan should be considered a transition land use with possible conversion to other land uses in the future.

6.2 ZONE CLASSIFICATIONS

The Zone Classification that may be applied by rezone action to properties within the Plan area designated Light Industrial and Industrial Reserve on the land use plan includes Light Industrial (M-1), Industrial Office Park (MP) and Industrial Reserve (IR). The Floodplain (F) combining zone classification must be applied to property that is subject to flooding. The Surface Mining (SM) combining zone classification is applied to specified properties. These zoning classifications on property within the Plan area, and in existence in the date of this plan adoption, are consistent with the Light Industrial or Industrial Reserve land use designation in this Plan.

6.3 DEVELOPMENT STANDARDS

The development standards for areas designated Light Industrial and Industrial Reserve as shown in Figure 3.1, Land Use Plan, are described in Title III, Chapter 25 of the Sacramento County Zoning Ordinance.

6.4 PERMITTED USES

Permitted uses for areas designated Light Industrial and Industrial Reserve as shown on Figure 3.1, Land Use Plan, are those that are permitted in accordance with the zoning for property in existence on the date this plan was adopted and those permitted uses described in Title II, Chapter 30 of the Sacramento County Zoning Ordinance, subject to the special conditions specified and contained herein. Notwithstanding the permitted uses associated with the zoning on property in existence when this plan is adopted remains in effect until the property is rezoned.

6.5 FUTURE USE OF INDUSTRIAL LANDS

As noted in Section 6.1, the long term viability of industrial uses for all of the lands designated in the Land Use Plan is questionable due to existing land uses and physical constraints. Some portions of the designated industrial area may be considered for a change of land use in the future. If that occurs, property owners will be invited to participate in any future master planning efforts to re-examine land uses within this area.

Open Space & Schools

7.1 PARKS AND PARKWAYS

This section of the Florin Vineyard Community Plan describes the park and open space system, which includes active parks, passive open space and natural areas, and parkways with off-street trails. The Land Use Plan shown in Figure 3.1 does not show specific park locations or sizes. Instead it shows the general locations of park sites proposed within concurrently processed tentative maps and a community park site proposed by Southgate Recreation and Park District. Two community parks are anticipated within the plan area to serve the larger community recreational needs. These park facilities are usually larger than 10 to 15 acres in size and may include various playfields, picnic facilities, community center buildings, and possibly pool and aquatic facilities. One possible community park location is on the west side of Gardner Avenue south of the existing church. This facility would connect and add to the existing Florin Community History Center and Sunrise Florin Park located on Fletcher Farm Drive to the west. A second community park is anticipated in the area east of Hedge Avenue, either north or south of Florin Road. This facility would serve community park needs for the eastern portion of the Plan area.



Several neighborhood parks are anticipated in the Plan area. These type of parks are usually 5 to 10 acres in size and may include playfields, playgrounds, court game areas, and picnic facilities. These parks serve residents within a radius of about 2/3 mile to 1 mile and meet the more immediate recreational needs of the neighborhood. One or more neighborhood parks should be included in the larger blocks of residential development in the Plan area. Exact location and design of neighborhood parks is subject to more detailed project layouts and is to be determined with individual subdivision applications. Small Mini-parks should also be included in project designs where appropriate. These parks are typically less than 5 acres in size and may include a children's play area, picnic tables, shade structures walkways and landscaping. Mini-parks are intended to provide a "green space" in areas of higher residential densities where a larger neighborhood park is not feasible.

Parks and open space within the Florin Vineyard Community Plan will be owned and maintained by the Southgate Recreation & Park District. Park facilities proposed within the Plan area must be reviewed by Southgate to insure consistency with the overall park and recreational needs of the community. Acceptance of parkland dedications is subject to approval by the Southgate Recreation and Park District's Board of Directors. Planning and construction of park and recreation facilities are usually completed by Southgate. Park construction is usually done after the basic site improvements are constructed as part of the subdivision improvements. Turn-key park development by subdividers can be done, subject to Southgate approval.

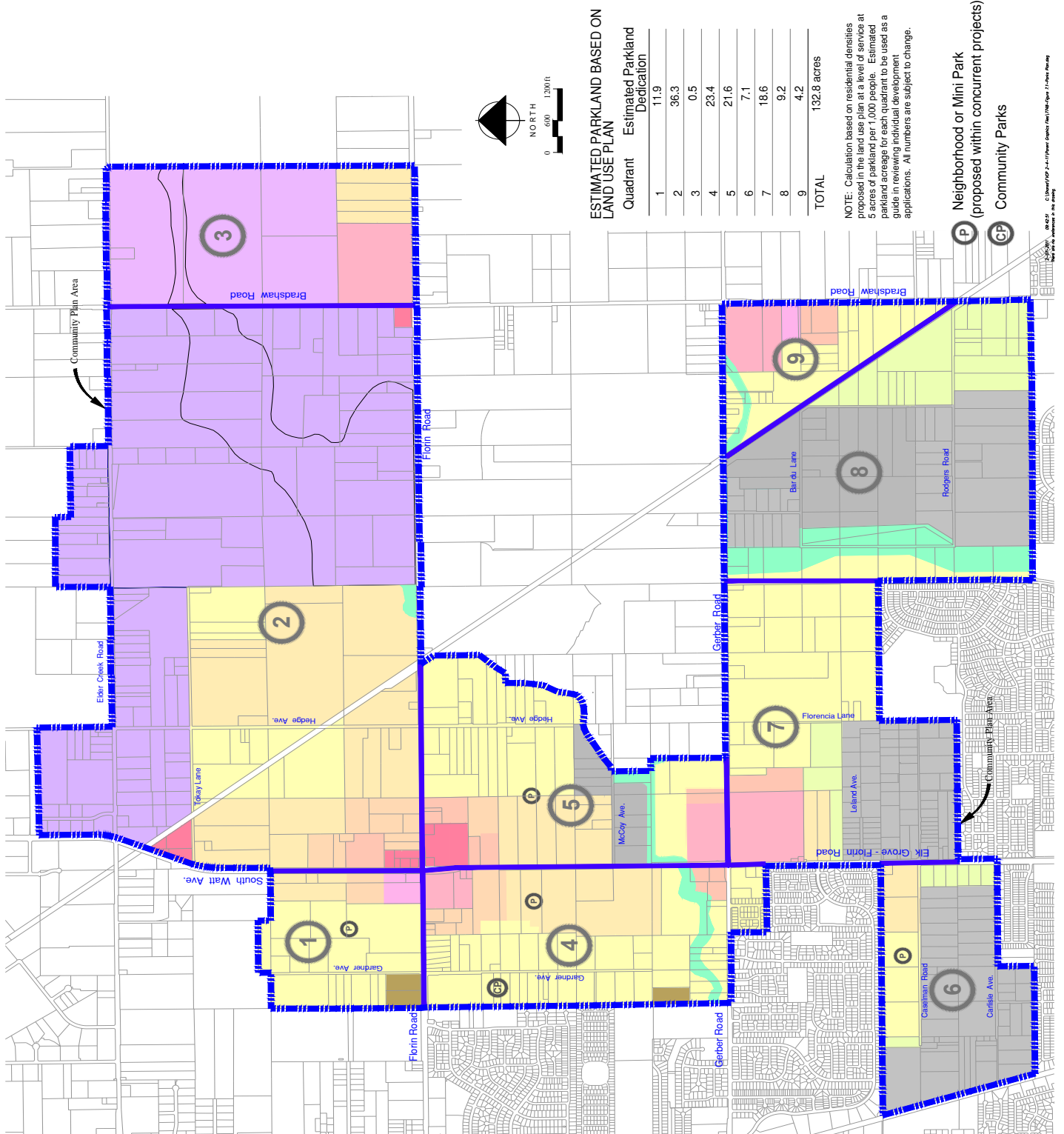


Figure 7.1 Parks Plan

Sacramento County's and Southgate's parkland dedication standard of 5.0 acres per 1000 persons is used with this Plan. Per-unit dedication factors for single family and multi family land uses is included in Section 22.40.035 of the Sacramento County Land Development Ordinance (Title 22). Park dedication can be applied to the range of park types described above. In some cases, a portion of the green street parkways, such as the 35-foot wide parkways planned for Gardner and Hedge Avenues (and potentially Florencia Lane) can be applied to the park dedication requirement. Medium density and multi-family projects may include private park facilities or on-site recreational facilities such as pools, tot-lots and club houses. These private facilities may be eligible for credit toward park dedication requirements per Sacramento County Title 22.

Figure 7.1, Parks Plan, illustrates estimated parkland dedication required for nine identified quadrants within the Plan area. The estimated parkland calculation is based on residential densities proposed in the adopted land use plan at a standard of 5.0 acres per 1,000 persons. The estimated parkland acreage required for each quadrant should be considered a guide in reviewing individual development applications. Actual acreage of parkland will vary depending on the size and location of proposed development. For example, smaller projects will likely pay parkland dedication in-lieu fees, while larger projects will likely dedicate land. Figure 7.1 shows park sites proposed within concurrently processed development projects and a community park proposed by Southgate. However additional park locations have not yet been identified for the remaining areas within the Plan area. Instead, parkland dedication requirements have been estimated for each quadrant. This is to allow flexibility in park site location as development occurs, and not predispose a park site use on particular properties.

Parkways within the Plan area are focused along the Gerber Creek and Elder Creek drainage corridors and the 400' to 600' wide powerline corridor east of Waterman Road and south of Gerber Road. These parkways will include pedestrian/bicycle trails with limited landscaping. The width of these parkways will vary based on regulatory agency requirements and the area of open space dedicated.

Parkways along the two drainage corridors and powerlines should be designed to be accessible to the adjoining land uses. Single loaded streets, open cul-de-sacs, or other open design features should be included along these parkways. Landscaping within these parkways should be visually interesting and natural in appearance with drought tolerant plantings. Section 9.4.2 of this Plan describes bicycle/pedestrian paths within these parkway corridors. A 10-foot paved path is planned within certain sections of the parkways described above.

7.2 DETENTION AND TREATMENT BASINS

A number of stormwater detention and treatment basins are proposed within the Plan area as part of the Florin Vineyard Drainage Master Plan. Detention basins are planned at the western end of three separate drainage sheds within the Plan area and range in size from 4 to 10 acres in size. Stormwater treatment basins are smaller (as little as one acre) and are more distributed in the Plan area. Location and design of these facilities are determined at the tentative subdivision map level and are not shown on the Florin Vineyard Land Use Plan.

The stormwater detention basins create an excellent opportunity to create additional open space within the community. Siting and design of detention basins should incorporate the surrounding neighborhood and provide passive open space opportunities where feasible. Stormwater detention basins within the Plan area should attempt to incorporate the following features:

- Detention basins should be located adjacent to active park facilities where possible to create a larger total open space area for the neighborhood. Paths within adjacent parks should connect into the detention basin area. Design of joint use facilities is encouraged to combine the needs of stormwater detention and active recreational areas. Joint use detention/park areas can include active sports fields but is limited to open turf or landscape areas with minimal hardscape or park improvements. Sacramento County Water Resources Division's guidance manual for detention/park joint use facilities provides more information.
- Single-loaded streets should be included at least one or two sides of a detention basin to create open access to the area. Single loaded street sections do not need sidewalks on along the detention frontage if a separate path is included with the open space.
- Sufficient area should be provided top of bank around the detention basin to allow for a pedestrian path, a post & cable or other fencing, and limited landscaping. Typically an area from 30 to 50 feet from top of bank is adequate setback for open space around the basin. A minimum setback of approximately 20 feet should be used where no trail is planned.
- A 10-foot pedestrian path of asphalt-concrete should be included within the top of bank area with connections to adjacent roadways at key intersections. A pedestrian path should be of sufficient distance to encourage passive open space use but does not necessarily need to encircle the basin.
- Varied shape and slopes of the basin should be designed to create a visually interesting open space area.
- Limited landscaping within the top of bank area including native drought tolerant trees and natural grasses. Shrubs and groundcover should be limited to an area 10 feet from a single loaded street.
- Benches and security lighting features should be incorporated into detention basin designs where feasible. Depending on size of the basin and alignment of pedestrian paths, benches or security lighting may be appropriate to provide a more desirable and safe pedestrian environment.

An illustration of possible stormwater detention basin and open space design that includes some of the above features is shown in Figure 7.2.



Figure 7.2 Detention Basin/ Open Space Illustrative

Water quality treatment basins are typically much smaller than detention basins. It may not be practical to include some measures described above such as paths, adjacent parks, varied shapes, or large setbacks. Where possible treatment basins should include single loaded streets and limited landscaping within the top of bank area.

7.3 WETLANDS AND NATURAL OPEN SPACE

Several areas within the Plan area may be preserved as open space for purpose of protecting wetland areas. Exact size, location and configuration of wetland preserve areas are determined on a project by project basis and is not shown in the Florin Vineyard Community Plan. Wetland preserve areas are regulated by State and Federal resource agencies and wetland permit requirements determine layout, ownership and ongoing maintenance. Wetland preserves are typically restricted to public access but still serve as a passive open space area for the community. Wetland preserves proposed within the Florin Vineyard Community Plan should include sufficient buffers between urban uses and wetlands, and single loaded streets or open neighborhood design. Sufficient area within wetland buffers should be provided to allow for pedestrian and bicycle trail/pathway improvements where feasible.

7.4 SCHOOL FACILITIES

The Florin Vineyard Community Plan is within the Elk Grove Unified School District (EGUSD), the boundaries of which cover a large portion of south Sacramento County. There are no schools within the Plan area at the time of plan adoption. Nearest schools include Maeola Beitzel Elementary in the Churchill Downs subdivision south of Gerber Road, Mary Tsukamoto Elementary located in the Vintage Park subdivision. Florin Elementary is located west of the Plan area and south of Florin Road, and Sierra Enterprise is located north of Fruitridge Road along Hedge Ave.

The Plan area is within three middle school/high school attendance boundaries at the time of plan adoption, however these boundaries are subject to change on an annual basis.

Depending upon which land use plan is adopted and based on student generation rates, EGUSD has determined that at least four elementary school sites are needed within the Plan area. There may be a need for a joint high school/middle school site in the plan area if residential development occurs as envisioned by the community plan. Siting of schools within the Plan area is constrained by setbacks from railroad, powerline corridors, gas stations, and liquor sale locations and other factors. Major roadways within the Plan area create 'quadrants' for logical elementary school sites. It has been determined that at least one elementary school should ultimately be located within each of the following general areas:

- Quadrant 1- west of Elk Grove-Florin Road/South Watt Avenue,
- Quadrant 2- south of Gerber Road and east of Elk Grove-Florin Road,
- Quadrant 3- East of Elk Grove-Florin Road, north of Gerber and south of Florin Roads,
- Quadrant 4- North of Florin Road and east of South Watt Avenue.

Specific school sites are not shown on Figure 3.1, Land Use Plan, and do not apply to particular properties within the Plan area. Instead, they are generally described in the four 'quadrant' areas shown on the Schools Plan (See Figure 7.3). This is to allow flexibility in school site location as development occurs within the Plan, and not predispose a school land use

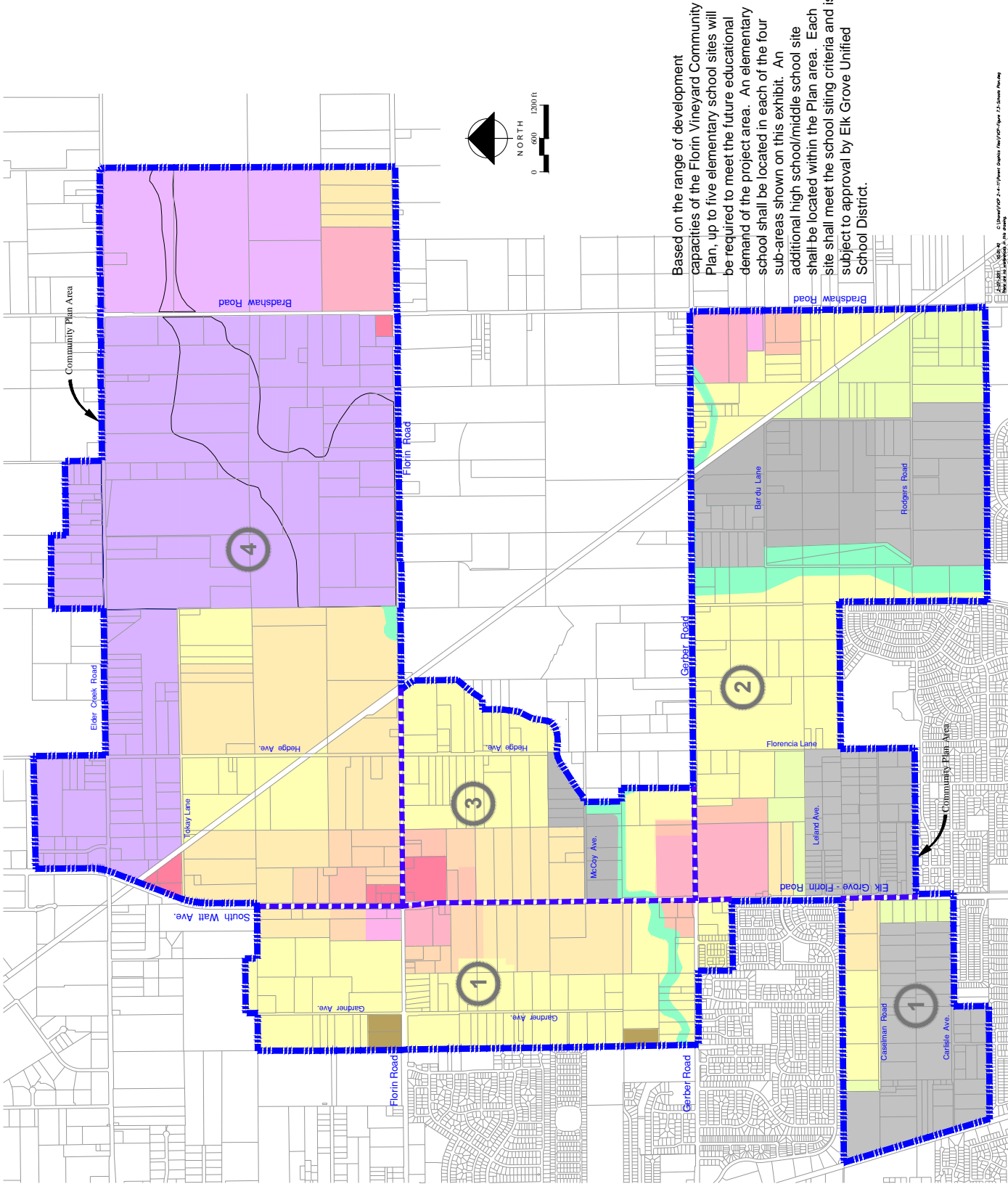


Figure 7.3 School Plan

on particular properties. However identification and evaluation of school sites shall be addressed via the subdivision map process or any other land use planning process used by the County for this purpose. Elementary school siting shall be resolved prior to approval of final maps for projects generating a demand for school facilities.

Detailed school location and configuration within projects is subject to EGUSD policies including those described in policy FV-41 and FV-42 contained with the Policy Plan section.

7.5 OPEN SPACE MANAGEMENT

Open space within the Plan area can take many forms, from parks and wetland buffers to small landscape strips along separated sidewalks. Ownership, maintenance responsibility and funding for maintenance vary depending on the type of open space area. Table 7.1 shows these aspects to open space areas within this Plan.

Component	Ownership	Responsibility	Funding Source
Major Roadways			
Landscape Corridors	SGR&P	SGR&P	L&L Dist. or M-R Dist
Sidewalks	SGR&P	SGR&P	L&L Dist. or M-R Dist
Soundwalls	SGR&P	SGR&P	L&L Dist. or M-R Dist
Medians	County	County	L&L Dist.
Interior Roadways			
Landscape Corridors	SGR&P	SGR&P	L&L Dist. or M-R Dist
Soundwalls	SGR&P	SGR&P	L&L Dist. or M-R Dist
Detached Sidewalks	County	County	Prop. Tax
Landscape Strips	Private	Private	Private
Attached Sidewalks	County	County	Prop. Tax
Drainage Corridors			
Channel Sections	SGR&P	SCWA	Utility Fund
Detention/Water Quality	SCWA	SCWA	Utility Fund
Buffer Areas	SGR&P	SGR&P	L&L Dist. or M-R Dist
Trails	SGR&P	SGR&P	L&L Dist. or M-R Dist
Misc Open Space			
Powerline Corridors	SGR&P	SGR&P	L&L Dist. or M-R Dist
Wetland Preserves	TBD	TDB	TBD
Parks	SGR&P	SGR&P	L&L Dist. or M-R Dist
Schools	EGUSD	EGUSD	Taxes & Developer Fees

Notes:

SGP&R = Southgate Recreation & Park Dist
 SCWA = Sacramento County Water Agency
 EGUSD = Elk Grove Unified School District
 L&L Dist. = Landscape & Lighting Assessment District
 M-R Dist = Mello-Roos Financing District
 Private = Adjacent private property owner

Table 7.1 Open Space Ownership, Maintenance, & Funding

7.6 ZONE CLASSIFICATIONS

The Zone Classification that may be applied by rezone action to properties within the Plan area designated Open Space on the land use plan includes Recreation ("O"). The (F) combining zone classification must be applied to property that is subject to flooding.

Zoning for parks, drainage channels and buffer areas, drainage detention & treatment basins, and wetlands and other open space areas should be zoned Recreation ("O"). Small parks, generally 3 acres & smaller, landscape corridors, parkways, & paseos, and powerline corridors should be zoned consistent with surrounding zone classifications.

Property that is identified for school facilities shall include a zoning classification that is compatible with the surrounding land uses. In the event the area shown for school facility is either reduced in size or eliminated, the underlying zone classification shall apply without the need for a rezone.

7.7 DEVELOPMENT STANDARDS AND PERMITTED USES

The development standards for Recreation ("O") zoning classifications in the Plan area are described in Chapter 20, article 3 of the Sacramento County Zoning Ordinance.

Design Guidelines

8.1 INTRODUCTION

Design Guidelines within the Florin Vineyard Community Plan are intended to insure quality development and implement land use policies found in this Community Plan and the Sacramento County General Plan. This chapter of the Community Plan shall supplement, not override, adopted County-wide design guidelines such as the Interim Multifamily Design Guidelines and the Commercial and Mixed Use Community Design Guidelines.

8.2 SINGLE FAMILY RESIDENTIAL DESIGN GUIDELINES

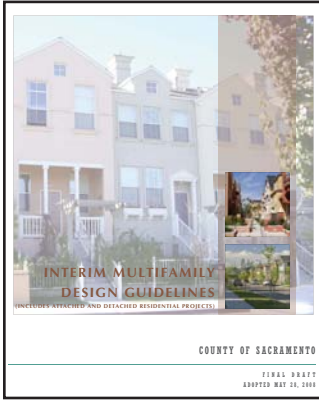
The County of Sacramento is currently preparing County-wide single family residential design guidelines at time of this plan adoption. Upon the completion and adoption of these County-wide guidelines, all future projects in the Florin Vineyards Community Plan will be subject to the requirements of the design guidelines program. In the interim, all proposed single family residential project will be subject to the following requirements.

Single family residential construction shall be subject to design review by the County prior to issuance of building permits. Non-discretionary single family residential projects requiring only a building permit are reviewed by the County Design Review Administrator (DAR). The Design Review Advisory Committee (DRAC) considers design review referrals from the DRA or requests from applicants. Single family home design shall incorporate the following design features;

- a. Architectural diversity within an established style is required within each single-family residential project. To accomplish visual diversity, architectural design, materials, and colors should vary. Architectural details, such as balconies, dormers, bay windows, recessed doors, changes in roof pitch, and porches are encouraged to further embellish basic architectural design.
- b. Attached garages should be offset from the main body of the dwelling to the maximum extent possible to improve the visual interest of the front elevation. House designs should emphasize the living space with front popouts, porches, or wide building-to-street orientation.
- c. House designs with side entry and limited visibility toward the street are discouraged.
- d. All buildings visible from an adjoining street should include architectural detailing, such as accent trim, reveals, recesses, projections, porches, and other similar features, which provide variety and visual interest.
- e. Roofs should employ a hip and gable design, with slopes of between 4:12 and 6:12. Flat roof designs should be avoided.
- f. Roof materials should consist of wood shakes, clay tile, composition, or other materials such as concrete or formed metal that have a similar color and texture.
- g. All roof materials and equipment should be non-reflective. Any alternative energy equipment should minimize reflective materials where possible.
- h. Roof-mounted equipment should not be visible from adjacent streets. Any alternative energy equipment should be kept low to the roof line.
- i. All wall and roof colors should be in muted colors.

8.3 MULTIFAMILY RESIDENTIAL DESIGN GUIDELINES

8.3.1 INTRODUCTION



The County adopted Interim Multifamily Design Guidelines shall apply to areas designated RD 7-12 and RD-20 on the Florin Vineyard Community Plan land use map or other areas of the plan proposed for multifamily housing. This includes small lot single family housing (7 to 12 du/ac. typical density), attached and detached townhouses (12 to 18 du/ac. typical density), and apartment/condominium housing (18 & above du/ac typical density). The Interim Multifamily Design Guidelines establishes “context type” standards for projects and relates project densities, setbacks, and building mass to the surrounding land use context. Since the adopted Guidelines sufficiently cover potential design issues in the Florin Vineyard Community Plan, they are incorporated with this Plan area. A limited number of special circumstances apply in

Florin Vineyard Community Plan and are discussed in the sections below.

8.3.2 “CONTEXT TYPE” APPLIED TO COMMUNITY PLAN, EXCEPTIONS TO THE GUIDELINES

The County Interim Multifamily Design Guidelines Section 1.6 describes “Community Context Types” for proposed projects and addresses project design in relation to the surrounding community. The guidelines describe three typical community context that applies to Category I, II, and III project types. The determined Context Type establishes development standards including density, setback, and building height.

The Florin Vineyard Community Plan contains a large range of land uses both within and adjacent to the Plan area. Several areas within the adopted Plan area are designated for agricultural-residential land uses. However in the future, these areas may choose to change to more intense land uses. It is possible that surrounding land uses such as agricultural or industrial may change land use designations in the future either via individual applications or new community planning efforts.

For these reasons, it is possible that the Context Type standards found in the Interim Multifamily Design Guidelines may require site designs that are rendered unnecessary with a land use change to adjacent properties. Further, the Context Type standards may require multifamily densities or development standards that are not consistent with General Plan or Community Plan goals and policies. Based upon these circumstances, it is recognized that the Context Type development standards found in the Interim Multifamily Design Guidelines may not apply to all projects in the Florin Vineyard Community Plan and exceptions to the requirements may be approved based upon particular circumstances of individual projects.

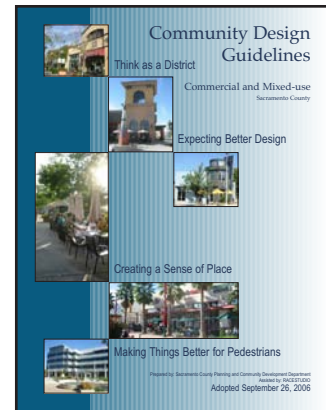
8.3.3 DESIGN REVIEW PROCESS

Multifamily design guidelines shall be implemented through the standard development plan review process in Sacramento County. Projects should be reviewed in relation to the Interim Multifamily Design Guidelines, the Florin Vineyard Community Plan, and other applicable documents.

8.4 COMMERCIAL/ MIXED USE DESIGN GUIDELINES

8.4.1 INTRODUCTION

The County adopted Commercial and Mixed-use Community Design Guidelines shall apply to areas designated Commercial, BP, M-1 and IR on the Florin Vineyard Community Plan land use map or other areas of the plan that are proposed for commercial or industrial development. The adopted guidelines cover areas of site and architectural design, signage, and village centers. Since the adopted Guidelines sufficiently cover potential design issues in the Florin Vineyard Community Plan, they are incorporated with this Plan area. A limited number of special circumstances apply in Florin Vineyard Community Plan and are discussed in the sections below.



8.4.2 COMMUNITY FOCUS AREAS

The two major intersections of Florin / Elk Grove-Florin and Gerber / Elk Grove-Florin Road create community focus areas and opportunities for creating community themes as discussed in Section 8.5 below. The majority of commercial land use designations in this Plan are located at these intersections. It is important to incorporate design features within commercial projects that help create community identity and focus for this portion of south Sacramento County. The adopted Commercial and Mixed-use Community Design Guidelines includes a section on "village center districts" and many of the guidelines in this section apply to in-fill and mixed-use commercial projects and may not apply to commercial projects in this Plan area. However, parts of this section, such as streetscape and landscape, lighting, may be applied to commercial projects within the two community focus areas where appropriate.

Gerber / Elk Grove-Florin Road- The two commercial land use designations at this intersection shown on Exhibit 3.1, Land Use Plan, provide a strong sense of place for this part of the community. Site plans for development of these commercial areas should incorporate the following design elements;

- Corners should be designed consistent with the Primary Community Gateway features described in Section 8.5.3 and should include a major pedestrian connection developed to connect the two properties. Any building pads developed at the corner should be related strongly to the corner with any supporting parking placed in the rear or side.
- A major access point to both sites along Gerber Road shall be developed approximately 1,000 feet east of the intersection with Elk Grove-Florin Road. This access point shall provide for both vehicle and a major pedestrian crossing between the two sites.

- The public and private property space along Gerber Road from the Elk Grove-Florin Road intersection to the access point approximately 1,000 feet east should be developed as a major landscape element giving special identity to this node. Landscape trees within the median strip and along the private property frontage should be consistent. Continuous double row of trees should be used where possible. Tree types should maximize decorative character and also provide visibility of the commercial building facades.
- Some building or landscape elements shall be provided near this crossing on each parcel to reinforce the sense of connection between the two properties.
- Common elements such as street, parking, pedestrian lighting, seating, arbors and kiosks, trash receptacles should be coordinated for both parcels to provide some joint design continuity and relationship.
- The main commercial buildings developed on the two properties should provide for a major focus on Gerber Road with the intent that the two developments appear to relate strongly to each other and create a unified community focus area.
- Large parking areas shall be broken up into “outdoor rooms” using landscaping and required tree cover in a strong design pattern. Some pedestrian paths from Gerber Road to the main commercial areas shall be emphasized with landscape or constructed elements.
- Building facade designs should have some consistent relationship to the overall community focus area but each property need not replicate the other. The same applies to development within each property.

8.4.3 OTHER COMMERCIAL AREAS

Other areas of commercial land use, particularly along Bradshaw Road, are more isolated and adjacent to other adopted planning areas. The viability of creating community focus in these areas is limited. Quality design principals should be applied and the “commercial districts” section of the Commercial and Mixed-use Community Design Guidelines appropriately applies in these cases.

8.4.4 INDUSTRIAL USES

The industrial land use designations in this Plan are limited in intensity and may be converted to other land uses in the future. The adopted Commercial and Mixed-use Community Design Guidelines do not specifically address industrial development. Instead, Zoning Ordinance requirements apply. Development of industrial projects shall be consistent with policies of this Plan and the County General Plan. Design review of industrial projects should consider the possible land use conversion of other industrial lands within the Plan.

8.4.5 DESIGN REVIEW PROCESS

Commercial and Mixed-use design guidelines shall be implemented through the standard development plan review process in Sacramento County. Projects should be reviewed in relation to the adopted Commercial and Mixed-use Community Design Guidelines, the Florin Vineyard Community Plan, other applicable documents.

8.5 COMMUNITY THEMES

8.5.1 INTRODUCTION

The Florin Vineyard Community Plan area is fragmented in shape and is bounded on several sides by previously approved planning areas. However the Community Plan maintains community structure with the major roadway alignments including South Watt Avenue, Florin Road, Gerber Road, Elk Grove-Florin Road, and Bradshaw Road. These major roadways create identity for the community. For example, the design of landscape and walls along each of these major roadways will establish identity for various parts of the community. In addition, the intersections of Florin / Elk Grove-Florin and Gerber/Elk Grove-Florin create community focus areas in the Florin Vineyard Community Plan. The intent of this portion of the document is to establish standards and guidelines that will create identity for the major roadways and focus areas within the Plan area.

The County adopted Commercial and Mixed Use Design Guidelines includes a section on “village center districts”. Many principals described in this section, such as streetscape and landscape, lighting, and signage apply to creating community themes described in this section and should be applied where appropriate.

8.5.2 MAJOR ROADWAY STREETSCAPES

The design of major roadway streetscapes (4-lane and larger) is an essential visual and physical element to create visual interest throughout the community. Major roadway streetscape includes median, landscape and other pedestrian amenities such as seating, lighting and pedestrian furniture. The guidelines presented here work concurrently with the roadway designs seen in Section 9.0, Circulation.

Consistency with the major roadway streetscape theme is an important factor. Design of the streetscape for Elk Grove-Florin Road, for example, should be consistent within major road segments to insure compatibility of individual development projects within the Plan area. Major roadway segments can be defined by intersections with other major roadways. For example, Elk Grove-Florin Road could have three streetscape themes, one below the intersection of Gerber Road, between Gerber and Florin Roads, and north of Florin Road. Not all streetscape segments must be different, and not all segments must be the same. Rather, a range of streetscapes should be used where appropriate for the particular circumstances of each segment. Streetscape compatibility with surrounding existing or planned projects is important. For example, the Gerber Road corridor has the planned North Vineyard Station Specific Plan to the north, and Elk Grove-Florin Road (south of Gerber Road) has the existing Tamarindo and Vintage Park subdivisions.

This community plan document does not include detailed streetscape themes described above. Rather, this document includes design guideline concepts that will be applied as projects proceed to the construction design phase. It is premature to define design details with community plan adoption when questions such as phasing, timing, and design trends can change over time. Instead, the design of major roadway streetscape segments should be established prior to approval of construction drawings for development projects within a particular segment.

8.5.3 GATEWAYS & ENTRIES

Community gateways provide a sense of arrival to neighborhoods and define boundaries between areas of greater and lesser intensity. The Florin Vineyard Community is integrated into a mix of surrounding existing or planned neighborhoods. As such, creation of community gateways at the Plan area boundaries is not appropriate. For example, the southern portion of Elk Grove-Florin Road has segments in and out of the Plan area and a community gateway would be confusing. Instead, community gateway statements at the major intersections of Elk Grove-Florin/Gerber Roads, and Elk Grove-Florin/Florin Roads better defines the community areas and corresponds with the community focus areas described in other Section 8.4.2 of this Plan.

Primary project entries are anticipated along the major roadways and will vary in number and location along each segment. Primary entries may include or exclude medians as appropriate. Primary entries should be treated with similar materials, colors, and forms as the landscape theme established for the major roadway segment. Figure 8.1 illustrates location of primary and secondary community gateways. Possible village entries and corridor walls are also shown.

Landscape design plans and specifications for primary and secondary community gateways, village entries, and corridor walls and fencing shall be reviewed and approved by Southgate Recreation and Park District, if they are to be owned and/or maintained by the District.

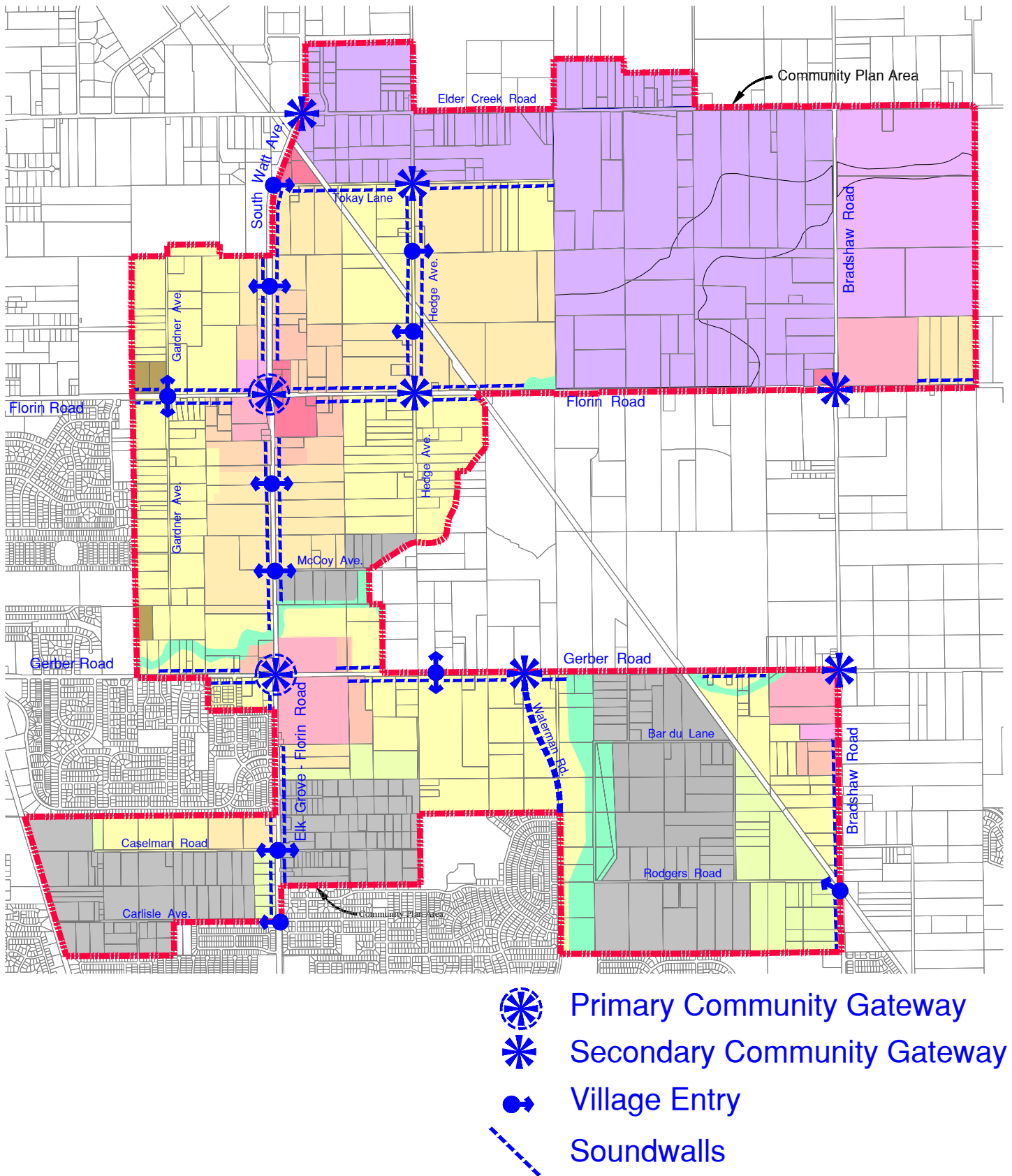


Figure 8.1 Gateways and Entries Plan

Primary community gateways are located at the two community focus areas within the Plan. They define the two key points of the Florin Vineyard community and maintain the highest level of identification. As a visual focus point, primary community gateways include a vertical element of stone clad pilaster and landscape elements to define a portal to the community. The primary community gateways should be designed to meet the following criteria:

- Primary community gateways will use consistent materials, colors, and forms that provide visual continuity to the area.
- The ground plane will be highlighted with colored and textured paving integrated with the gateway landscape design.
- Stone clad pilaster shall include community signage as described in Section 8.5.6 of this plan. Project specific signage may be included on monumentation walls.
- Lighting and artistic elements may be integrated into the gateway monuments to help create quality visual character.
- Include art in public places, preferably oriented toward history of the Vineyard community, where possible.

Figure 8.2 shows the primary community gateway elements to be applied at the major intersections identified in Exhibit 8.1.



Figure 8.2 Primary Community Gateways

At the intermediate level, secondary community gateways identify transitions between neighborhoods and contribute to establishing an overall community theme. Secondary gateways are located at major intersections away from the to community focus areas. These gateways include the same design theme as the primary community entries but contain less focused vertical element and landscaping detail. Primary gateways should define the immediate neighborhood but not distract from the primary community gateways found at the community focus areas. Figure 8.3 illustrates the secondary community gateway concept.

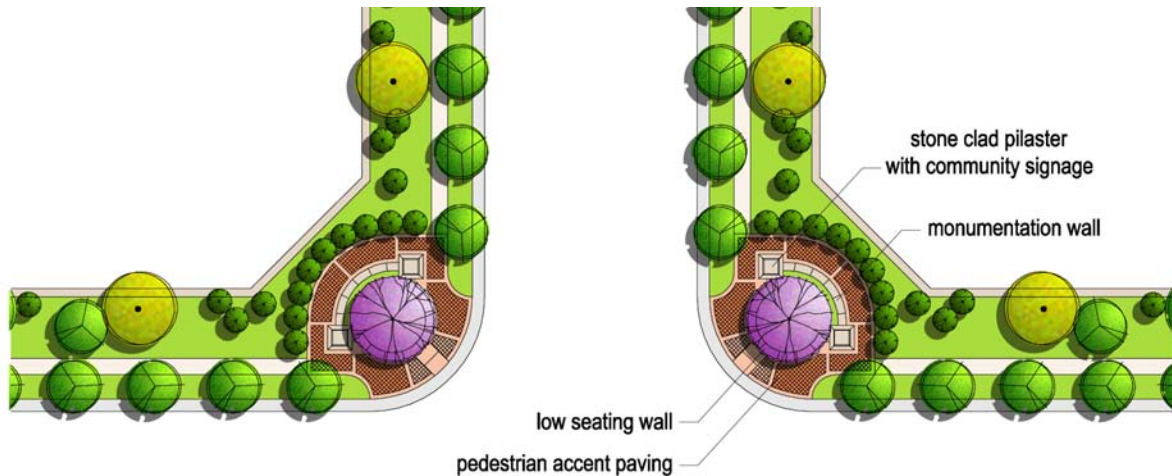


Figure 8.3 Secondary Community Gateways

Village entries are used at intersections of major roadways and residential project entries. Intersections with collector streets or primary residential streets will use the village entry element. These entries will define residential neighborhoods and include quality landscape design that is consistent with the primary and secondary community gateways. Village entries have the option to include an entry island. Depending on intersection design and need for left-hand turn pockets, the feasibility of an entry island will vary. No community signage is needed at village entries. Neighborhood signage is appropriate on the low stone wall. Figure 8.4 shows the village entry concept.

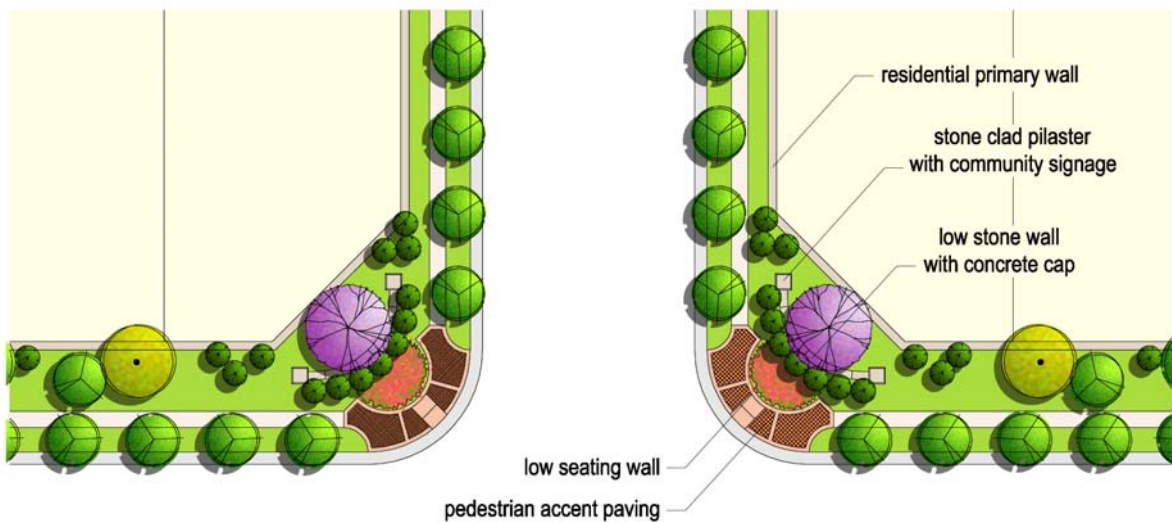


Figure 8.4 Village Entries

8.5.4 MONUMENTATION & SIGNAGE

The primary and secondary community gateways described above should be attractive to the viewer and establish a distinct identity for the community. Gateway monumentation and signage should be compatible at each corner of the major intersections with similar materials, colors, and forms.

The Florin Vineyard community has a history founded in agricultural practices. Dominated by grapes and later by strawberry crops, the area's strong history should be recognized in the community monumentation. To implement this, consistent community signage will be included in the stone clad pilaster at the primary and secondary community gateways. Figure 8.5 shows three options for community signage. This signage shall be included with the stone clad pilaster in the primary and secondary community gateways.



Figure 8.5 Community Signage

Project signage is appropriate within the primary and secondary community gateways to establish project identification. Commercial and residential monument signage should be located on the monumentation wall and not conflict with other elements of the community gateway. Illuminated project signage is prohibited. Additional project signage may occur further from the major intersection, preferably near a project driveway.

8.5.5 SOUNDWALLS & FENCING

Soundwalls and fencing define an important part of the streetscape. Soundwalls and fencing along major roadways within the Plan area shall be designed consistently and complement landscape design within the corridors.

Soundwalls along major roadways shall include split faced block of masonry, precast concrete Proto II System or similar material, and split faced block pilaster on minimum 150 foot spacing. Graffiti-resistant coatings shall be used. Color shall be Basalite Standard Tan, D345 or equivalent. Wall manufacturer and/or color can deviate from those described in this plan, however any alternatives shall be consistent with the guidelines within this Plan. Figure 8.6 illustrates the required design elements for soundwalls. Soundwalls should incorporate breaks at street intersections (with appropriate wraparound), open-end cul-de-sacs, and park, trail, or open space access points.

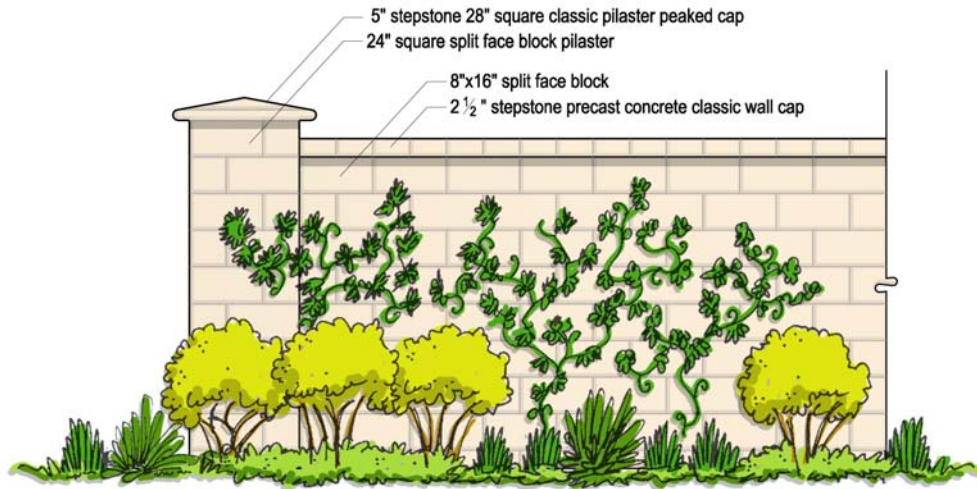


Figure 8.6 Soundwall Theme

A split rail fence shall be used at the primary community gateways. Short segments of split rail fence are encouraged at other locations within secondary community gateways and village entries or landscape corridors to continue the fencing theme throughout the community. Figure 8.7 shows the split rail fence theme with low-end pilaster.

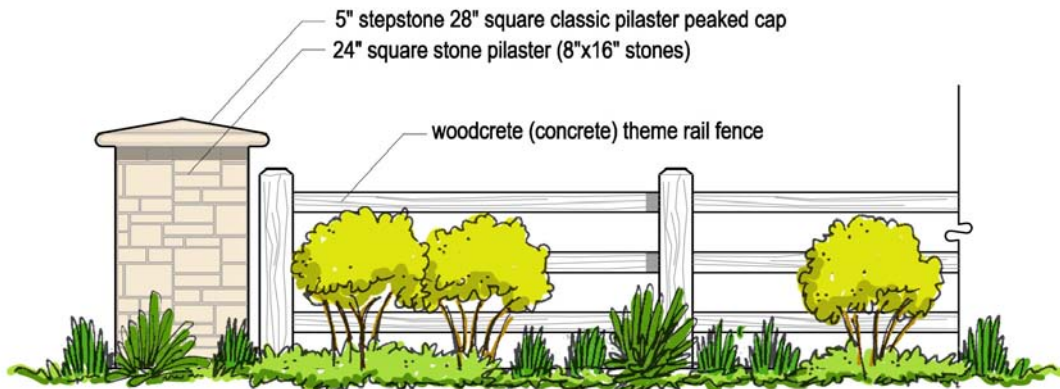


Figure 8.7 Split Rail Fence Theme

Soundwalls shall comply with standards found in County Code Section 301-66, "Soundwalls Adjacent to Streets", and height requirements found in the Florin Vineyard Community Plan Environmental Impact Report (EIR) or subsequent environmental documents. Soundwalls and fencing within landscape corridors to be owned and/or maintained by Southgate Recreation and park District shall comply with the District's improvement requirements.

Design Guidelines

Soundwalls along major roadways may be avoided next to medium density residential and high density residential projects where on-site building mass or patio wall construction allows for appropriate noise mitigation. This variation creates variety along the major roadway streetscape and increased visual interest. For security reasons, a 6-foot tubular metal fence may be needed along the rear of landscape corridor to define the public and private space. Fences shall be placed along property lines to avoid any potential nuisance or property owner dispute.

8.5.6 LANDSCAPE CORRIDORS & PLANTINGS

Landscape corridors along major roadways within the Plan area shall be designed consistently as described above. Each of the five major roadways in the Plan area should be designed with a consistent landscape theme of primary and secondary street trees. Major roadways (4-lane and 6-lane) will include a 25-foot landscape setback on both sides of the street and a 12-foot planted median. Landscape corridors will include deciduous and evergreen trees and drought-tolerant shrubs and groundcovers. Rows of shade trees with drought-tolerant shrubs and groundcovers should be planted in the major road medians. Use of irrigated turf is limited to that of an accent or entry statement.

Design of major roadway landscape corridors will be maintained by the Southgate Recreation & Park District. Landscape design plans for these corridors must be consistent with these guidelines, and meet the operations and maintenance needs of Southgate.

8.5.7 COMMUNITY STREET LIGHTING

Lighting levels within the Plan area shall meet requirements of the Sacramento County Improvement Standards. Lighting along roadways shall be sufficient to create safe pedestrian and bicycle use. Energy efficient street lighting shall be used as required by the Climate Change Plan approved as part of this Community Plan.

Street lighting standards for poles and lights are set forth in the County Improvement Standards. Typical standards for local residential streets include 20 foot poles with luminaires on top. These standards may be amended to include additional options for more decorative lighting standards. Decorative street lighting poles and luminaires are strongly encouraged. Examples of decorative street lighting include posts with smooth or ribbed steel, post arms, and luminaires using acorn, teardrop or el camino style heads.

Circulation

9.1 OVERVIEW

This section describes the planned roadway network, pedestrian and bicycle network, transit facilities and services for the Florin Vineyard Community Plan. The design of these facilities are critical components of the Community Plan and affect the quality and character of the community.

The Florin Vineyard Community Plan circulation system is founded on the 1-mile grid of existing County roadways. These include Florin Road, Gerber Road, Elk Grove-Florin Road, South Watt Ave, and Bradshaw Road. This grid system sets the foundation for a finer grid system of collector and primary residential streets. Policy FV-7 encourages a local street pattern within subdivisions that provide multiple points of access within a neighborhood and to surrounding commercial areas, parks, schools, and public transit.

Surrounding circulation facilities play an important role in the Florin Vineyard Community Plan structure. Bradshaw Road, a planned thoroughfare, connects Florin Road to Gerber Road and connects the eastern 'fingers' of the Plan area. Waterman Road is planned to curve north-south through North Vineyard Station and aids in overall circulation. The Gerber Creek and Elder Creek corridors continue through North Vineyard Station and create east-west trail corridors. The multi-powerline corridor east of Waterman Road runs through both Florin Vineyard Community Plan and North Vineyard Station and provides a north-south open space corridor. Finally, the California Central Traction Railroad traverses both planning areas and serves as both a circulation constraint and future trail opportunity.

The Plan Area includes Gerber Creek and Elder Creek corridors within the eastern and central portions. These creek corridors are planned to include a trail system that connects with neighborhood streets. This system is also part of the adjacent North Vineyard Station Specific Plan.

The plan boundary includes several tentative map applications processed concurrent with the Community Plan. These maps establish local street patterns internal to the project and provide street stubs to adjacent properties. Sufficient and logical street stubs are important to allow for interconnected street patterns in the future. Areas of the plan without concurrent tentative maps will need to continue the street patterns to insure consistency and connectivity.

9.2 GREEN STREETS FRAMEWORK PLAN

Early in the community plan process Southgate Recreation & Park District staff floated a circulation concept that was refined in conjunction with the Owners Group into what was coined the “Green Streets Framework Plan”. This plan included collector streets with landscape corridors, street hierarchy for internal subdivision streets, and pedestrian and bicycle paths of movement. The Green Streets plan continued with the system established in the adjacent North Vineyard Station Specific Plan.

The primary element of Green Streets are two north-south “Parkway I” streets, Gardner Avenue and Hedge Avenue. These streets are spaced 3/4 to 1 mile east and west of Elk grove-Florin Road, and serve as collector or primary residential roads to the surrounding neighborhoods. Both of these corridors are planned to terminate at the south end into the Gerber Creek and Elder Creek drainage corridors and open space trails.

An additional system of primary “Parkway II” streets is included in the Green Streets Plan. These streets form a smaller grid pattern within the neighborhood blocks and provide for enhanced streets encouraging pedestrian and bicycle movement. The combination of major thoroughfare/arterial roadways, collectors, Parkway I and Parkway II streets creates an appropriate hierarchy of street types and street patterns that creates connectivity within the many urban community blocks.

During the community review of the plan, it was evident that there is strong support for the construction of separated sidewalks to promote a strong walking environment within the plan area. Consistent with Sacramento County Improvement Standards, all collectors, arterials, and thoroughfares will have separated sidewalks. The plan also separated sidewalks for streets designated as Parkway I and Parkway II. In addition, separated sidewalks can be considered as an option during the review of development projects consisting of residential type streets.

Figure 9.1 shows the Green Streets Framework Plan for major roadways, Parkway I and II streets, and existing and planned signalized intersections. The exhibit also shows surrounding facilities in the North Vineyard Station Specific Plan and outside areas. The exhibit includes subdivision layouts and local street patterns based on applications that may have been revised or withdrawn. In addition some parks as shown may be changed. Figure 9.1 also shows existing signalized intersections and future signals that are conceptual and subject to change with future project review. For this reason, the Green Streets Framework Plan shows a conceptual street pattern, hierarchy and signalized intersections, not specific alignments or locations.

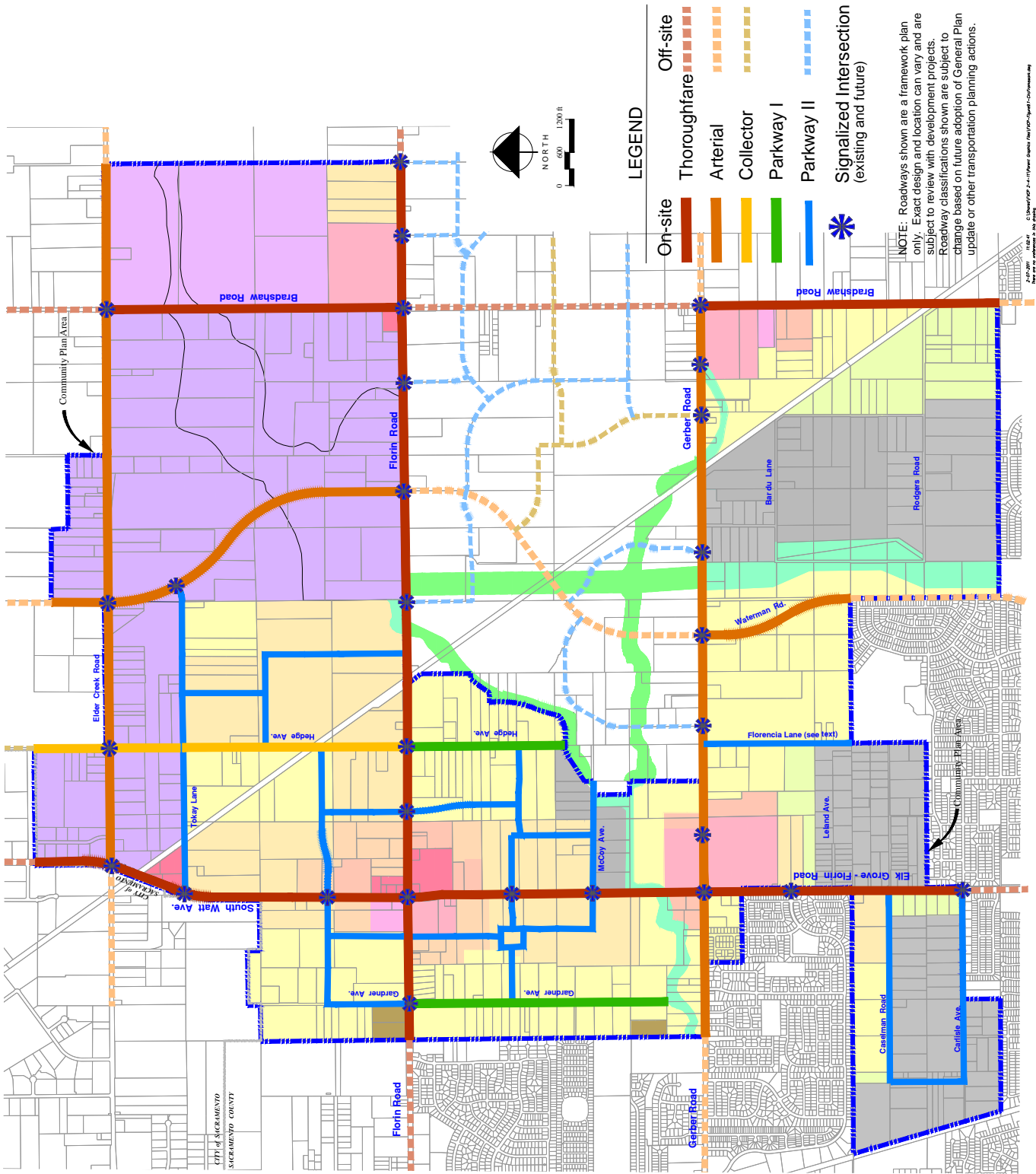


Figure 9.1 Green Streets Framework Plan

9.3 STREET SECTIONS

The Community Plan includes proposed street sections consistent with the intent of the Sacramento County Improvement Standards. Sections shown in Figure 9.2 for Thoroughfare and Arterial roadways serve as the major backbone for the circulation framework and are intended to match County Standards.



Figure 9.2 Thoroughfare & Arterial Street Sections

The Collector- Divided section shown in Figure 9.3 applies to the northern portion of Hedge Avenue from Florin Road to Elder Creek Road. This two-lane divided roadway is part of the Green Streets Framework Plan and includes 35-foot landscape on the east side and 15' landscape corridor on the west. The median divide allows for the anticipated higher traffic volumes along this section of Hedge Avenue and creates increased landscape amenity.

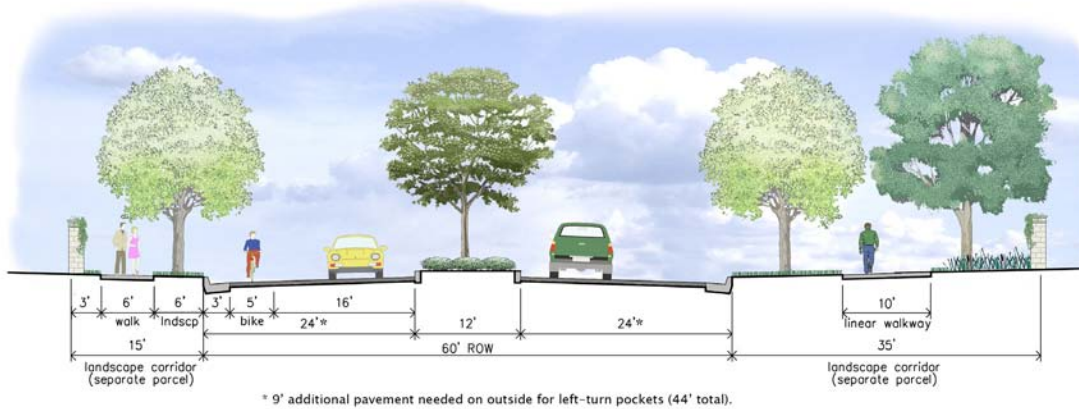


Figure 9.3 Collector Street - Divided Section

Circulation

The Parkway I section applies to portions of Gardner Avenue and Hedge Avenue south of Florin Road and is a key ingredient to the Green Streets Framework Plan (See Figure 9.4). The roadway section is based on the 42-foot wide Primary Residential right-of-way (back of curb). Parkway I includes Class 2 striped bike lanes. The parkway section includes a 35' landscape space with a 10-foot linear walkway. This green space runs north-south through the community and connects at the south end with the Gerber Creek/Elder Creek open space corridors. The parkway is located on the west side of Gardner Avenue and east side of Hedge Avenue. Connecting these public spaces with the north-south corridors is a key ingredient of the Green Streets Framework Plan. Other side of the street is intended for front-on and/or side-on lots with a separated walk and enhanced 8-foot mow strip with street trees. On-street parking is proposed on the residential side of the Parkway I section but is prohibited on the parkway side. Florencia Lane (south of Gerber Road) is designated a Parkway II roadway. Ultimate designation of Florencia Lane is subject to change. If a school and/or park facility is located along the alignment of Florencia Lane, then Florencia shall be designated for as a Parkway I roadway.

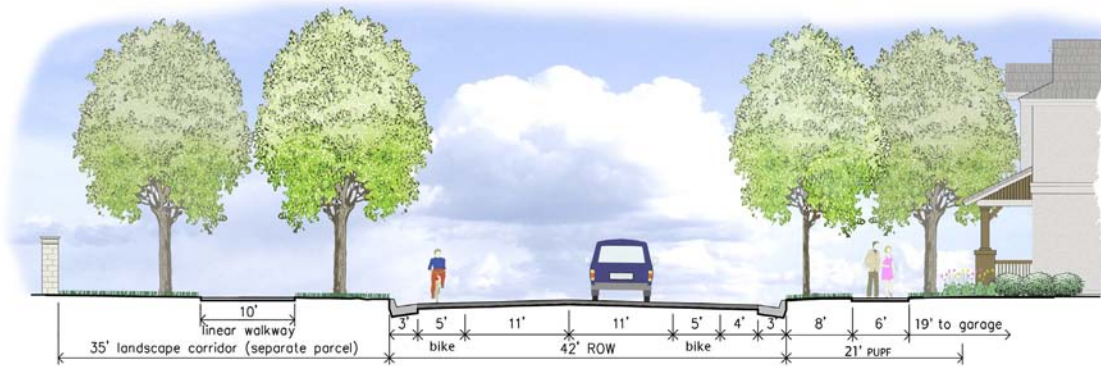


Figure 9.4 Parkway I Section

The Parkway II section is shown in Figure 9.5 and serves as a primary street cross-connecting between larger streets in the neighborhood. For example, the Parkway II section cross connects the area between Garner Avenue, Elk Grove-Florin Road, and Hedge Avenue. The roadway section is again based on the 42-foot wide Primary Residential right-of-way (back of curb). Class III bike lanes are intended on these lower traffic volume roadways. Enhanced landscaping on each side includes a 6-foot separated sidewalk and a 7-foot mow strip with street trees.

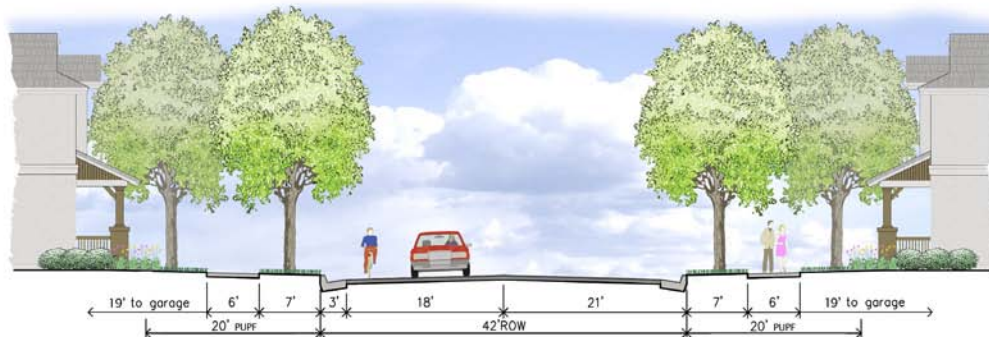


Figure 9.5 Parkway II Section

Primary Residential streets provide local circulation within neighborhoods that have greater traffic volumes and usually connect with the larger streets described above. Two types of Primary Residential street sections are included in this Plan. Figure 9.6 shows a Primary Residential Street with separated sidewalk. This section is intended to be used for Primary Residential streets that form a backbone of neighborhood circulation and should include the feature of a separated sidewalk. This street section adds to the Green Streets Framework Plan at the next level of street hierarchy. Figure 9.6 also shows a Primary Residential street section with attached sidewalk. This section applies to streets that meet the Primary Residential traffic volume criteria but do not need a separated sidewalk. Class III bike lanes are intended on these two roadways. Specific application of these two types of Primary Residential streets is more detailed than shown in the Green Streets Framework Plan and should be determined at the tentative subdivision map review stage.



Figure 9.6 Primary Residential - Separated and Attached Walk

The Minor Residential street section is shown in Figure 9.7 and is consistent with the intent of Sacramento County Improvement Standards for local public streets. This section is used for all local residential streets with low traffic, pedestrian, and bicycle volumes.

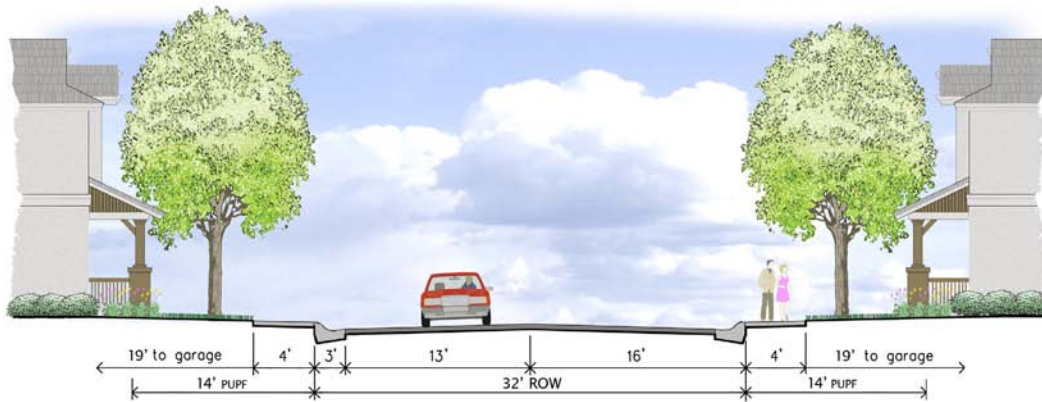


Figure 9.7 Minor Residential

9.4 PEDESTRIAN AND BICYCLE TRAIL SYSTEM

Providing a safe and inter-connected pedestrian and bicycle system within the Florin Vineyard Community Plan is an important part of the community design. A viable and sustainable plan for pedestrian and bicycle circulation is critical to achieving goals of the County General Plan and this Community Plan. A series of pedestrian and bicycle trail paths is a related part of the Green Streets Framework Plan described above. Figure 9.8, Pedestrian and Bicycle Trail Plan shows the Bicycle and Trail Plan for the Florin Vineyard area. Also included in Figure 9.8 are pedestrian and bicycle trail facilities approved as part of the North Vineyard Station Specific Plan.

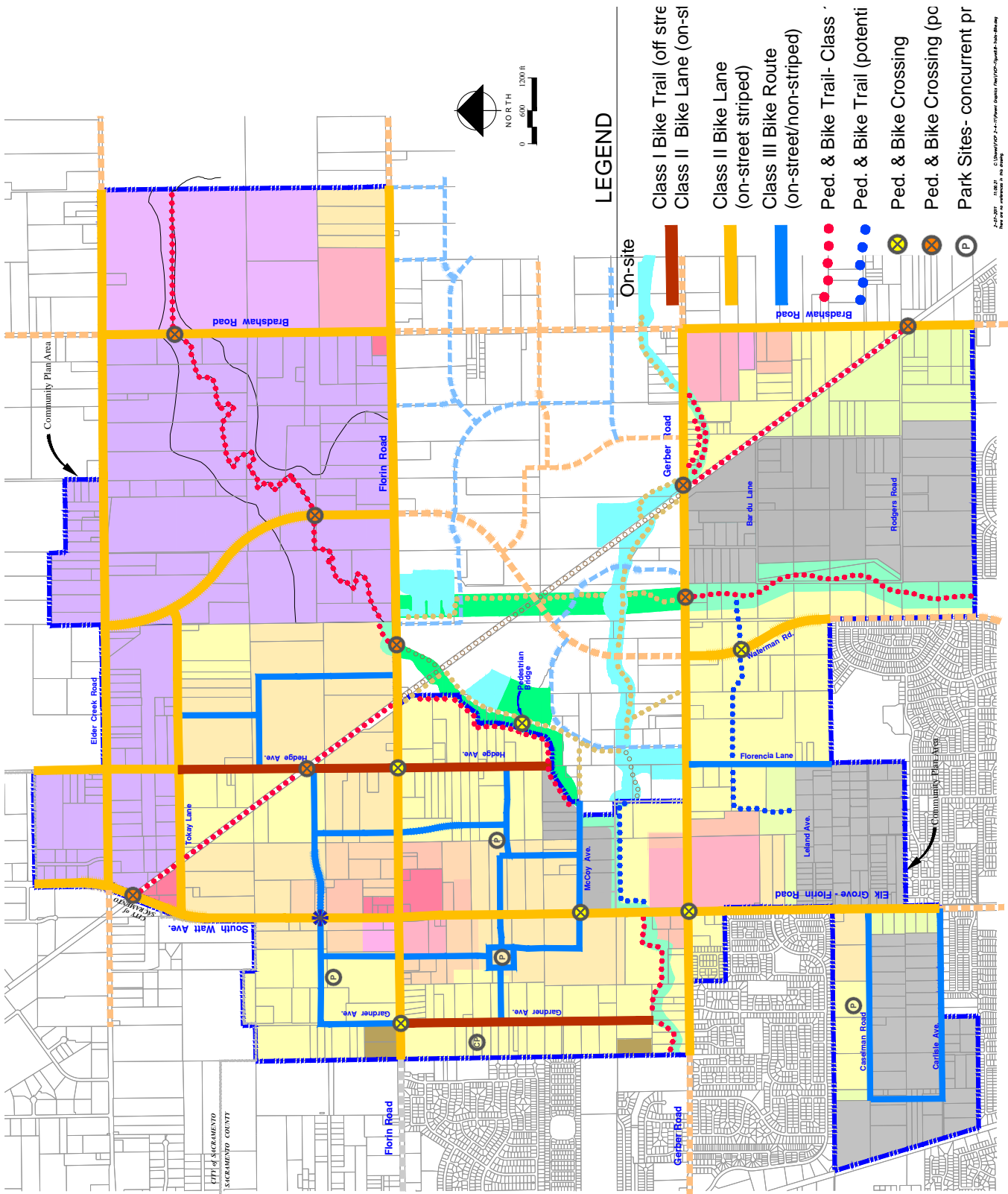


Figure 9.8 Pedestrian and Bicycle Trail Plan

9.4.1 PEDESTRIAN/BICYCLE PATHS ALONG ROADWAYS

Pedestrian/bicycle paths within the Community Plan follow the street hierarchy described above. The highest level includes major thoroughfare and arterial roadways within the Plan area. These four and six lane roadways include a Class II bicycle path and an 6-foot pedestrian path within the landscape corridor. These segments form a 1-mile grid pattern in the greater Vineyard area. These major roadways typically serve longer distance commuter bicycle use that favors Class II bicycle paths over a combined pedestrian/bicycle paths within landscape corridors. The intent is to separate the commuter bicyclist with pedestrians and recreational bicyclists.

The next level of pedestrian/bicycle paths are the Green Streets, Gardner Avenue and Hedge Avenue shown in the Parkway I street section (Figure 9.4). These streets form major north-south connections through the community and tie into Gerber Creek/Elder Creek open space corridors. These roadways include both a Class II bicycle path within the roadway and a 10' joint use path inside a 35' landscape corridor. Similar to the major roadways, this design allows for both commuter and recreational bicycle use in separate paths. Joint pedestrian/bicycle use of the 10' path is anticipated to be neighborhood or community level.

Parkway II streets (shown in Figure 9.5) provide pedestrian and bicycle paths in the form of Class III bicycle paths and separate sidewalks. These paths interconnect within neighborhoods from major roadways to the Parkway I roads. These corridors include front-on residential lots and lower traffic volumes, thus smaller bicycle and pedestrian separation is appropriate.

Figure 9.8 includes Ped. & Bike Trail (potential) in several areas of the plan. These paths are a second tier below the Class 1 bike lanes or pedestrian walkways. These paths are intended to provide a basic path of movement through an area and can consist of a normal attached, or detached, sidewalk along roadways adjacent to an open space area. Where no roadway is proposed, a minimum 6 foot asphalt path should be used. Two Class 1 Ped. & Bike Trails are shown outside the plan area in the North Vineyard Station Specific Plan and include the south side of Gerber Creek from the Champions Golf Course east to Passallis Lane and along the Central Traction Railroad. These connections are not currently included in the North Vineyard Station circulation or open space plans, but should be included in future improvements, if feasible.

Ped. & Bike Crossings shown in Figure 9.8 are shown at two levels. Yellow crossing symbols illustrate ped & bike crossings at known road intersection locations. Orange (potential) Ped. & Bike Crossing symbols identify general areas for roadway crossings, but exact location and configuration need to be refined once intersection locations and adjacent project layouts are defined.

9.4.2 OPEN SPACE TRAILS

Trails within the Florin Vineyard Community Plan are focused on the Gerber/Elder Creek open space corridors which run along the east side of the plan boundary and then west near Gerber Road. An integral part of the system are the trails approved as part of the North Vineyard Station Specific Plan. These trails are shown in Figure 9.8, Pedestrian-Bicycle Trail

Plan, and run along the east side of Elder Creek, connecting to Florin Road at the railroad tracks, and south side of Gerber Creek, connecting to Gerber Road via "1" Street, within North Vineyard Station. The Florin Vineyard Community Plan adds to this system by connecting to the Elder Creek path via McCoy Avenue out to Elk Grove-Florin Road, then south to Gerber Creek on the west side. A trail is planned along the north side of Gerber Creek west to Gardner Avenue. This trail system connects Gardner Avenue with Hedge Avenue which is part of the Green Streets Framework Plan. Another trail is planned along the powerline corridor east of Waterman Road and south of Gerber Road. This corridor is 400 to 600' wide and is defined by three 230KV electric transmission lines. A 10' pedestrian/bike path is planned running north-south within the open space corridor shown on the Land Use Plan. This path connects north of Gerber Road in the North Vineyard Station area. A pedestrian/ bike trail is shown along Elder Creek within the industrial area north of Florin Road. A general connection should be made along this corridor, however exact location and configuration with surrounding land use is subject to change based on ultimate land uses and proposed development projects in this area.

Off-street pedestrian/bike trails within the Florin Vineyard Community Plan shall consist of a 10' asphalt path with 2-foot shoulders on each side as shown on Figure 9.9.

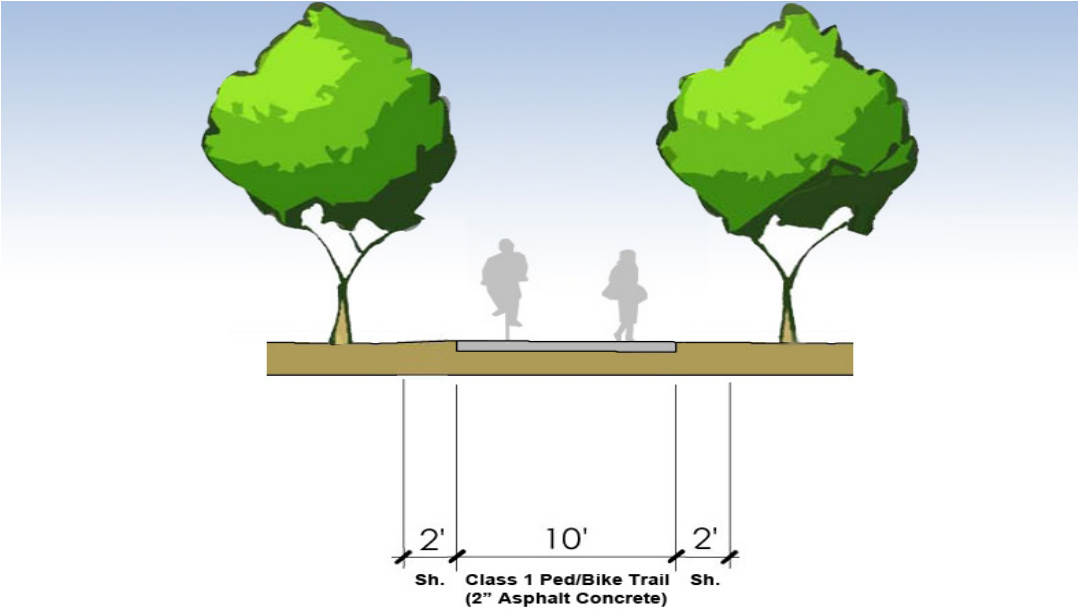


Figure 9.9 Pedestrian/Bicycle Trail Section

9.5 TRANSIT SYSTEM

Public transit services within the Florin Vineyard Community Plan area will be provided by Sacramento Regional Transit District (RT). RT operates the light rail service along Folsom Boulevard, located approximately 3 miles north of the Plan area boundary. At the time of Plan adoption, RT does not provide transit service within the Plan area, however services are anticipated along the major roadways. Local and express bus service along Bradshaw and Elk Grove-Florin Road is anticipated in the future. Local bus service on Gerber Road and Florin Road should be anticipated.

Circulation

The Sacramento County General Plan (2007 draft) identifies transit facilities within the Plan area. Bus Rapid Transit (BRT) routes are planned with mixed use lanes on Elk Grove-Florin Road and Florin Road. BRT with exclusive lanes is planned along Bradshaw Road.

Transit facilities within the Plan area will include bus stops and shelters located at intersections of major roadways. These major intersections correspond with community focus areas described in the Design Guidelines section of this Plan. Increased densities of commercial and residential land uses will promote transit use to and from these areas. Intersections of major roadways and neighborhood entries should also include bus stops. The exact location and design of transit facilities is to be determined with review of individual development applications. The Land Use Plan includes increased densities within 1/4 mile of Elk Grove-Florin Road which will encourage transit use.

9.6 RAILROAD FACILITIES

The Central California Traction Company (CCTC) railroad bisects the plan area from the northwest to southeast. The tracks are at grade (not elevated) and typically consist of a 100 foot wide right-of-way corridor. At grade crossings of all public roadways exist along this rail line. Use of this line has varied historically. At time of Plan adoption, this line is used for freight car storage. No trains were running along this line. The CCTC tracks create a constraint to local circulation within neighborhoods. Particularly in the area north of Florin Road, the CCTC tracks limit roadway connections that would interconnect neighborhoods on the east and west side of the tracks.

Future use of the CCTC railroad line is uncertain. Continued use as a freight rail line is possible but is unlikely unless demand increases with industrial land to the north within the City of Sacramento. The California High Speed Rail Authority has identified a possible route from Downtown Sacramento south to Merced and Los Angeles that includes the CCTC railroad line. However no decisions have been made and the Sacramento route is planned for later phases of this high speed rail project. At the other end of the spectrum, the Central Valley Rails to Trails Foundation has identified the CCTC route from Elder Creek Road south to Lodi as a possible conversion to pedestrian, bicycle, and equestrian trails. The CCTC railroad line is shown as a future ped. & bike trail in Figure 9.8. This is consistent with trail designations in the County-wide Bicycle Master Plan.

The Florin Vineyard Community Land Use Plan covers the above range of railroad uses. If freight train operations occur, soundwalls and buffers will be needed for residential development along the right-of-way. Project designs similar to approved projects in North Vineyard Station will be needed in this case. Possible high-speed rail use would require noise and buffering beyond the capability of adjacent properties due on the routing question and long timeline for possible construction. Added measures will likely be required from any high-speed rail proposals. A possible trail system would best complement the surrounding neighborhoods. Placement of parks and other future, possible connections to the CCTC tracks will allow for trail connections, should the rails to trails effort become a reality.

9.7 PHASING OF TRAFFIC MITIGATION

Appendix D, Phasing of Roadway Improvements, sets forth the detailed requirements for mitigating the traffic impacts associated with the development of planned land uses within the Florin Vineyard Community Plan. These impacts are presented in full detail in Appendix I of Volume 3 of the Florin-Vineyard GAP Community Plan Project Final Environmental Impact Report (Control No. 04-GPB-CPB-0096).

Phasing of Traffic Mitigation Roadway Improvements

The County Department of Transportation conditions, as described in Appendix D, are based on traffic mitigation measures identified in the Final Environmental Impact Report (FEIR) for the FVCP and on additional traffic analysis to determine the appropriate phasing of roadway improvements associated with development of approved land uses in the FVCP. For a complete description of the mitigation measure improvements, refer to the FVCP FEIR. These conditions apply to all residential development within the FVCP area and are in addition to any other conditions applied to individual properties within the FVCP. Non-residential development projects associated with retail/commercial services, office, and industrial land uses within the FVCP are not subject to these phasing conditions, but shall contribute appropriately to meeting overall FVCP traffic mitigation obligations through construction of onsite and adjacent roadway improvements (including logical segments) as specified in project conditions of approval and by payment of Florin Vineyard Fee Program Roadway Fees and Sacramento County Transportation Development Fees.

The Land Division and Site Improvement Review (LDSIR) Section in the Department of County Engineering will maintain an inventory of the cumulative total number residential units that may be constructed on lots created by final subdivision maps within the FVCP. Such potential residential construction will include single family detached and attached residential units and multiple family residential units of all types. For multiple family parcels the unit count will be based on approved site plans where applicable as well as zoned density and dwelling unit limits. The tally of total cumulative residential units will be posted on the LDSIR internet website and updated regularly.

Required installation of the specific roadway improvements listed in each numbered condition in Appendix D applies when the specified residential unit threshold for that section is reached (e.g., Items 1 through 5 prior to 451 residential units, Items 6 and 7 prior to 766 residential units, Items 8 through 11 prior to 826 residential units, etc.). Property owners with approved tentative subdivision or parcel maps with entitlements for residential development should consult with the Department of Transportation staff to determine which traffic mitigation measures must be constructed before their final maps may be recorded. Collaboration among owners seeking to develop their properties is anticipated to achieve timely installation of these roadway improvements. Coordination of planned public construction and private construction of mitigation measures will be encouraged. The subsequent conditions explain the overall scope and design requirements of each specific roadway improvement, as well as referencing the FEIR Mitigation Measure that the improvement satisfies or partially satisfies.

Conditions that specify certain roadway improvements required to mitigate Cumulative + Project traffic impacts be constructed by developers on a "fair share basis" are associated

Circulation

with more flexible rules regarding scope of roadway improvements, consideration of interim improvements, assembly of needed financing from multiple sources, and the specific phasing threshold. The language in each of these conditions includes the phrase “on a ‘fair share’ basis satisfactory to the Department of Transportation.” The Director of Transportation shall determine at the time additional final map residential units are proposed if the then current and anticipated traffic levels of service associated with the mitigation improvement are critical, if the contributions toward funding needed for construction of fair share improvements are sufficient to enforce the indicated phasing threshold, if FVCP development should be required to construct the entire fair share improvement or some lesser portion of it, and if alternative improvements can be substituted to effect a comparable reduction in traffic congestion.

To the extent feasible during the process of implementation of the FVCP Public Facilities Financing Plan, the traffic mitigation phasing obligations associated with the North Vineyard Station Specific Plan (NVSSP) will be integrated with these FVCP roadway phasing requirements to achieve a “blended” Roadway CIP and fee program applicable to developments in both the FVCP and the NVSSP. Attachment D may be amended from time to time by the Board of Supervisors after appropriate CEQA review if deemed necessary to, inter alia, integrate traffic mitigation phasing obligations with the NVSSP, or based on the results of future traffic studies.

Administration

10.1 DEVELOPMENT APPLICATION REVIEW

Development within the Plan area is subject to approval of subsequent entitlements by the County. Subsequent approvals may include but are not limited to rezones, tentative subdivision maps, conditional use permits, variances, and development plan reviews. Individual project applications will be reviewed to determine consistency with the Florin Vineyard Community plan and other regulatory requirements.

Individual development projects within the Plan area are subject to review and approval of subsequent entitlements by the County of Sacramento. Application and processing requirements shall be in accordance with the Sacramento County Zoning Code and other regulations, unless otherwise modified by this Community Plan. All subsequent development projects, public improvements, and other activities shall be consistent with this Community Plan and all other applicable County policies and standards. In approving subsequent projects and permits, the County may impose conditions as reasonable and necessary to ensure that the project complies with the Community Plan and other applicable plans and regulations.

Individual development applications may be filed with the County after approval of the Community Plan. The exception is applications filed within the two 'windows' for concurrent processing as provided for in adopted Resolution 2004-0578. County staff will review subsequent applications for consistency with the Community Plan. Applications must demonstrate consistency with policy language, land use designations and circulation plans, development standards and design guidelines, other relevant implementation documents.

10.2 ENVIRONMENTAL REVIEW

County staff will review all individual project applications to ensure compliance with CEQA requirements. The Environmental Impact Report (EIR) for the Florin Vineyard Community Plan serves as the base environmental document for subsequent entitlements. Development applications will be reviewed on a project-by-project basis to determine applicability of the Community Plan EIR.

In general, if it is determined that a subsequent project is consistent with the Community Plan, review of project environmental impacts will incorporate consideration of Section 15183 of the CEQA Guidelines for determining the type of environmental document for the project. Environmental review of subsequent projects will include use of the Community Plan EIR and use tiering of environmental documents per Section 15152 of the CEQA Guidelines.

10.3 MAJOR AND MINOR AMENDMENTS

An amendment to the community plan may be initiated by a property owner's application or at the direction of the Board of Supervisors. Amendments shall be consistent with the requirements of the Sacramento Zoning Code and California Government Code. Community Plan amendments shall not require a concurrent general plan amendment unless it is determined by the Planning Department that the proposed amendment would specifically conflict with the general plan.

Amendments to an adopted Specific Plan can be categorized as either minor or major. This determination is to be made by the Planning Director or his/her designee. Those amendments considered major will be processed as discretionary entitlement applications similar to specific plan amendments. Minor amendments can be reviewed and acted upon by the Planning Director with appeals directed to the Planning Commission and Board of Supervisors. Amendments to the Community Plan can include but are not limited to changing land use designations, design criteria, development standards, or policies. The Planning Director shall determine the limits and applicability of any proposed amendment to the Community Plan.

Major community plan amendments to the Community Plan may include but are not limited to:

- Significant changes to the distribution of land uses which may substantially affect the key planning concepts set forth in this Community Plan.
- Significant changes to the arterial street system that would substantially alter the land use or circulation concepts set forth in this Community Plan.
- Changes to the design guidelines and/or development standards which, if adopted, would substantially change the physical character of the Plan Area as envisioned by the Community Plan.

Minor community plan amendments may be reviewed and acted upon by the County Planning Director or Zoning Administrator, and shall be considered minor when it is determined that it does not have a significant impact on the character of the Plan. Decisions of the Planning Director or Zoning Administrator may be appealed to the appropriate review authority in accordance with standard appeal procedure. The Planning Director shall make a written determination as to whether or not a requested amendment is minor based upon the following criteria:

- That the proposed adjustments to the design guidelines or development standards are offset by the merits of the proposed design and not significantly change the anticipated physical characteristics, goals, and intent of the Community Plan;
- Proposed changes to the alignment of major or smaller streets would not substantially alter the land use or circulation concepts set forth in the Community Plan;
- Proposed changes to land use diagram shapes or to the alignment of collector or secondary streets would maintain the general land use pattern and/or provide an improved circulation system consistent with the intent and direction of the goals and policies of the Community Plan;

- The proposed change is not expected to increase environmental impacts beyond the levels identified in the EIR;
- The proposed change would not result in an increase in the total maximum number of units approved in the Community Plan and will comply with the criteria for modifications of land use diagram and density adjustments; and
- No formal application or environmental review is required for minor community plan amendments. No hearings with the Planning Commission or Board of Supervisors is required unless the findings of the Planning Director or Zoning Administrator is appealed.

10.4 MITIGATION MONITORING

CEQA requires all State and local agencies to establish and monitor programs for projects approved by a public agency whenever approval involves adoption of either a mitigated negative declaration or specified findings related to environmental impact reports. For Sacramento County, the appropriate department for establishing and maintaining this program is the Department of Environmental Review (DERA).

The Mitigation Monitoring and Reporting Program (MMRP) is indented to satisfy the requirements of CEQA as they relate to the final EIR prepared by DERA for the Florin Vineyard Community Plan. This monitoring program is intended to be used by County staff and project developers in ensuring compliance with adopted mitigation measures during project implementation.

MMRP's will be included with individual project applications and may include measures adopted with the EIR for the Florin Vineyard Community Plan. The Environmental Impact Report for this Plan addresses mitigation measures on a program level. As a result, there is no MMRP adopted with this Plan. Instead, individual development project applications will be analyzed for potential environmental impacts by DERA and propose mitigation measures on a project specific level

